



LAND USE COMMITTEE
OCTOBER 3, 2016
NOON
COUNCIL CHAMBERS

1. **WOODLAND DISTRICT DESIGN GUIDELINES**
RYAN ANDREWS, PLANNING MANAGER
(STAFF REPORT ATTACHED)

2. **HOUSEKEEPING ORDINANCE FOR 2016 COMPREHENSIVE PLAN AND DEVELOPMENT REGULATION UPDATE**
RYAN ANDREWS, PLANNING MANAGER
(STAFF REPORT ATTACHED)



LAND USE COMMITTEE
October 3, 2016

SUBJECT: Woodland District Design Style Guidelines

RECOMMENDATION: The Committee will be briefed on the Planning Commission Recommendation. No action is needed. The recommendation will be schedule for consideration by the City Council at a public meeting.

STAFF CONTACT: Scott Spence, City Manager *SS*
Rick Walk, Community Development Director *RW*
Ryan Andrews, Planning Manager *RA*

ORIGINATED BY: Community Development Department

ATTACHMENTS: 1. [Woodland District Design Style Guidelines](#)
2. [Draft Amendment to LMC 16.24](#)
3. [Comment E-mail from Zach Kosturos](#)

FISCAL NOTE: None.

PRIOR REVIEW: The Woodland District Form-Based Code was adopted by the City Council on February 25, 2016.

BACKGROUND:

As part of the scope of work for the Woodland District Form-Based Code, the consultant team prepared a package of materials to inform the development community about the new FBC, associated procedures, and requirements. As part of this package, the Woodland District Style Guidelines were developed to provide additional guidance for staff and the development community on the codes expectations for architectural style and materials.

The Woodland District Form-Based Code contains specific details when it comes to the public realm. The details include requirements for the location of buildings, setbacks, stepbacks, modulation, articulation, pedestrian and vehicular connectivity, and street design. However, the code lacks specificity when it comes to the use of materials to create that “northwest look and feel” established by the Woodland District Strategic Plan. The Guidelines provide details for building material palette, glazing, entrance canopies, and

other details that will articulate the vision for the district. Unlike the Woodland District Form-Based Code, which is very prescriptive, the Guidelines allow flexibility and creativity in their application to each individual project. Although the guidelines will not be codified in the Lacey Municipal Code, they will apply to development projects as regulations and will be adopted by the City Council.

As part of the amendments, a minor code amendment has been drafted for Lacey Municipal Code 16.24 (the Woodland District Form-Based Code), which would require that all projects in the Woodland District comply with the Design Style Guidelines.

The Planning Commission conducted a public hearing on the Woodland District Style Guidelines on September 20. Notification of the public hearing was sent to all members of the public who have previously participated in the Woodland District planning process as well as published in The Olympian. The City received one written comment, which is attached. No verbal comments were received at the hearing. At the conclusion of the hearing, the Planning Commission voted unanimously to recommend adoption of the Woodland District Style Guidelines and associated amendment to LMC 16.24 to the City Council for adoption.

ADVANTAGES:

1. The Guidelines will provide additional detail and guidance for the use of building materials to articulate the vision for the Woodland District.
2. The visuals used in the Guidelines help better define the “northwest look and feel” and contemporary nature of the District identified in the Woodland District Strategic Plan.

DISADVANTAGES:

1. None identified.



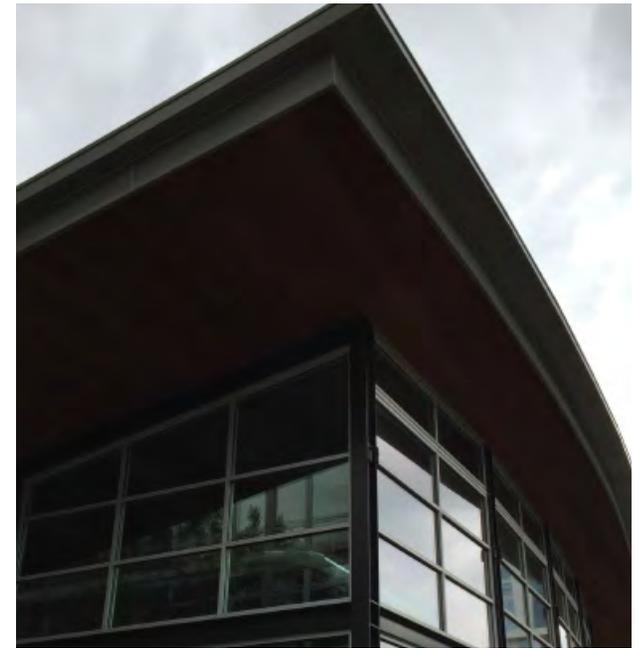
The Pacific Northwest Architectural Style Guidelines and Design Character Elements

Strong Simple Forms

Urban places such as the Woodland District are dynamic environments that are active throughout the day. The design emphasis should be on the quality of the public places where activities of daily living take place, rather than on overly complicated building surfaces. Emphasize the spaces created by building massing through strong simple forms.

This guideline may be accomplished by

- » Strong, simple forms that reflect the building's function and use.
- » Strong low-slope projecting roof forms.
- » Building scale, proportions, and design expression that acknowledge the existing context.
- » Outdoor rooms are accessible from the building but also provide privacy from the sidewalk.
- » Strong, clear definition of the building roof cornice against the sky.
- » Visually lighter materials are supported by denser, heavier materials; e.g., wood, metal, and stucco over masonry or concrete.



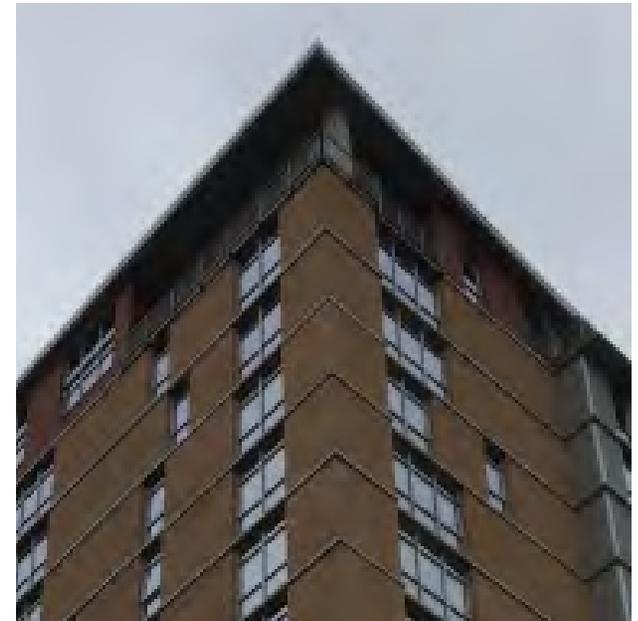
Projecting roof form accentuates building top



Change of form and material at corner entrance



Semi-protected outdoor courtyard



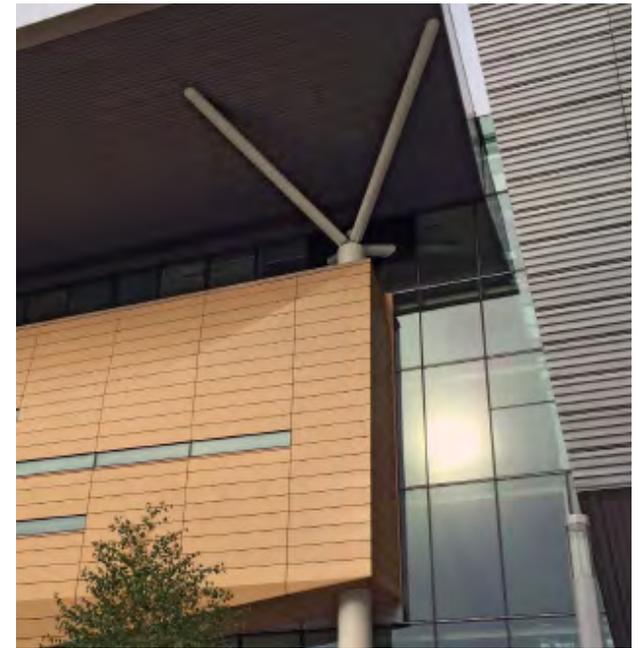
Irregular roofline contrasts simple building form

Material Expression

The strong simple forms of buildings are best supported with the integration of a limited set of carefully detailed materials.

This guideline may be accomplished by

- » A limited palette of materials that are simply used, but with careful detailing. Such materials may include: concrete, wood, standing-seam sheet metal, glass, board-and-batten, wood siding, corrugated sheet metal, board-formed concrete, board-and-batten siding, commercial/ industrial terra cotta, stone siding, spaced wood sun screens, ply-formed concrete, horizontal wood siding, brick, sheet metal panels, standing-seam metal roofing, stucco, heavy-timber.
- » Structural systems are expressed as design elements, which could include exposed columns and beams or other visible structural frameworks.
- » Application of materials creates visual variation and patterning of the façade.
- » Plants and landscaping that act as soft material to complement building palette.
- » Materials that emphasize strong simple forms and building massing.



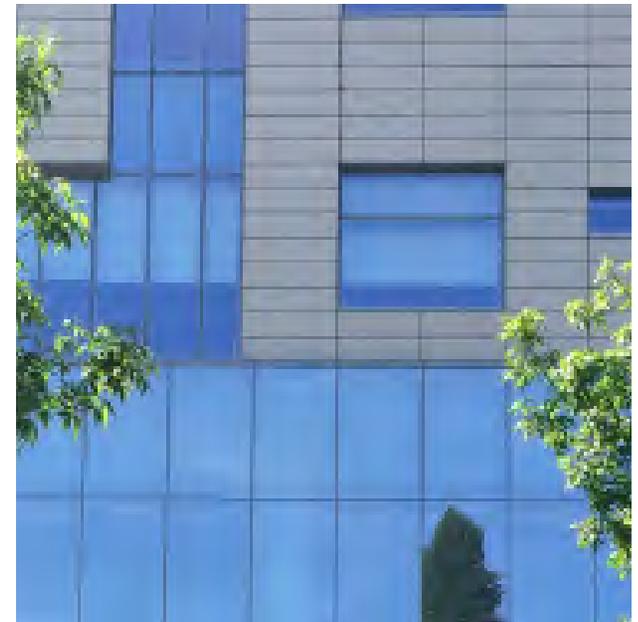
Tube steel columns, glass, metal panels



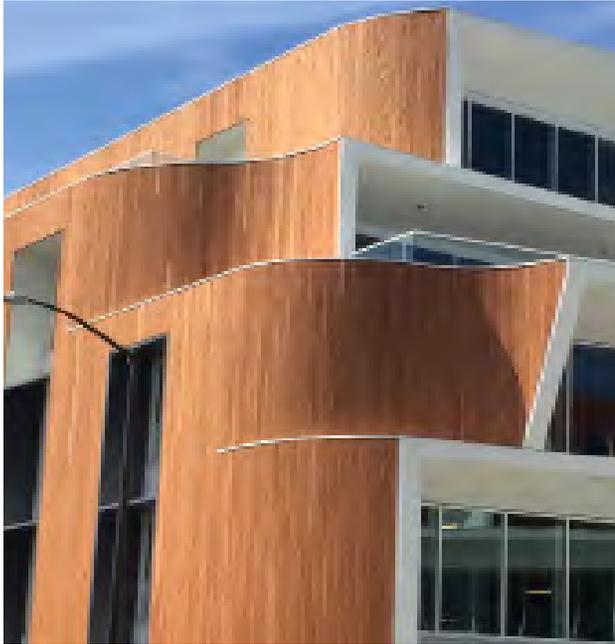
Light stucco and glass against dark brick



Cast in place concrete, aluminum panels and glass



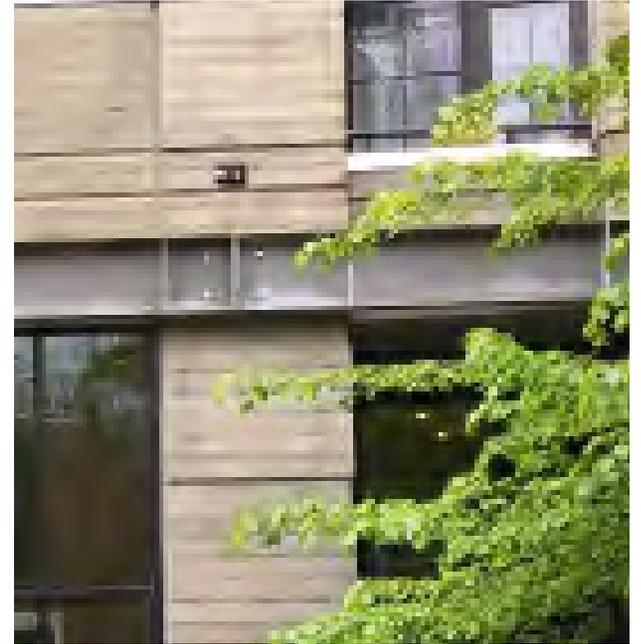
Glass and aluminum panels



Vertical wood planks, stucco, glass



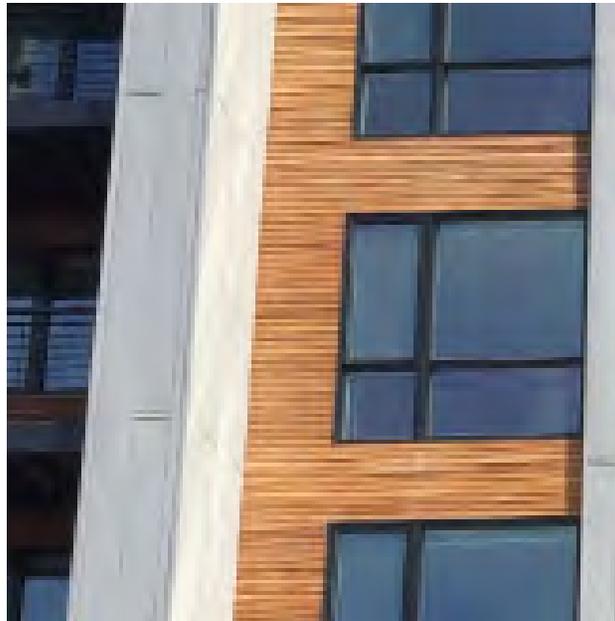
Brick, stucco, glass



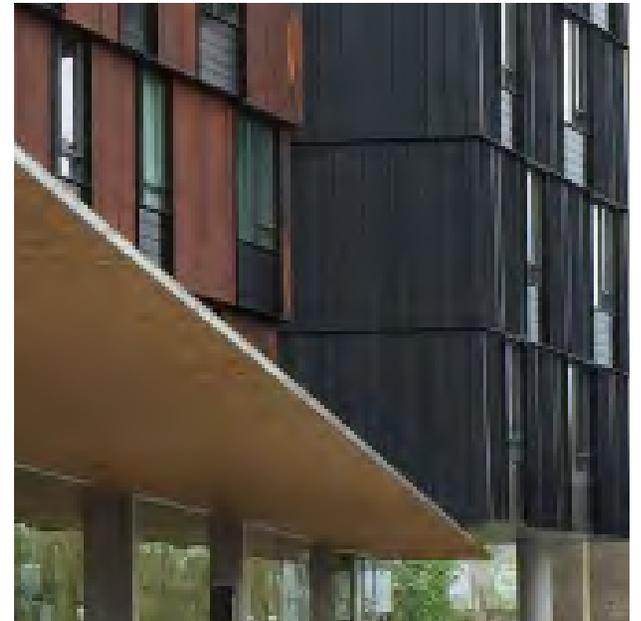
Board formed concrete creates a horizontal pattern



Cast-in-place concrete is softened with green planting



Metal panels, horizontal wood planks, glass



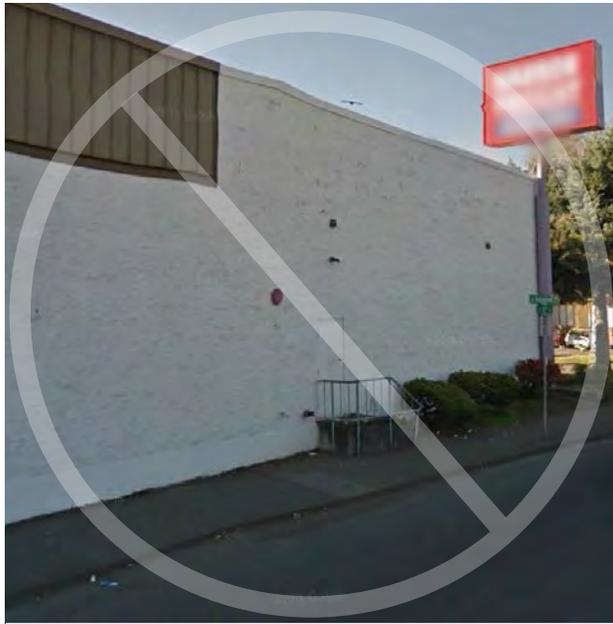
Vertical and horizontal metal panels, wood soffit

Material Expression, Continued

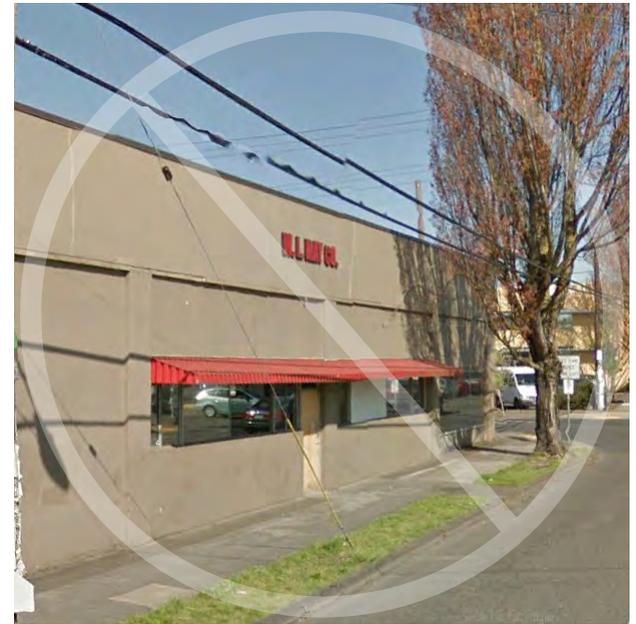
- » Use of multiple materials is required; don't rely on one material alone.
- » The primary material must be penetrated with openings and meet all of the other guidelines for glazing, canopies and detailing.
- » Prohibited materials include vertical T-111 siding, metal siding and stucco as primary material (accent only).



Undesirable material use



Undesirable material use



Single material creates unwanted pedestrian experience

Generous Use of Glazing

The community vision for the Woodland District is as a welcoming center of Lacey. Abundant glazing that connects the street to the interior of buildings reinforces the warm sense of community that extends from the public realm into businesses, offices and homes.

This guideline may be accomplished by

- » Generous use of commercial/ industrial glazing that connects the interior function to the public realm.
- » Glazing that helps to define the programmatic functions of different parts of the building and can be understood from exterior and interior.
- » Additional glazing that helps to define points of entry at each level.



First floor glazing, bold glazing patterns above



Generous storefront glazing invites cafe seating



Vertical glazing delineates building's program



Ground floor residential windows behind setback

Carefully Detailed

The emphasis for the Woodland District is on walking and the human scale of the individual. While building massing and form may be simple, the details of a building—how one material meets another—are extremely important. Pay specific attention to all of the surfaces that people touch in the course of their transit through the Woodland District.

This guideline may be accomplished by

- » A balance of horizontal and vertical lines and materials that add interest.
- » Building forms look simple at a distance, with details that become apparent as the observer grows closer to the building.
- » Clear articulation of the building wall at the roof.



Careful detailing where wall meets roof



Subtle undulating form at building corner above step back



Multiple concrete treatments structure landscaping



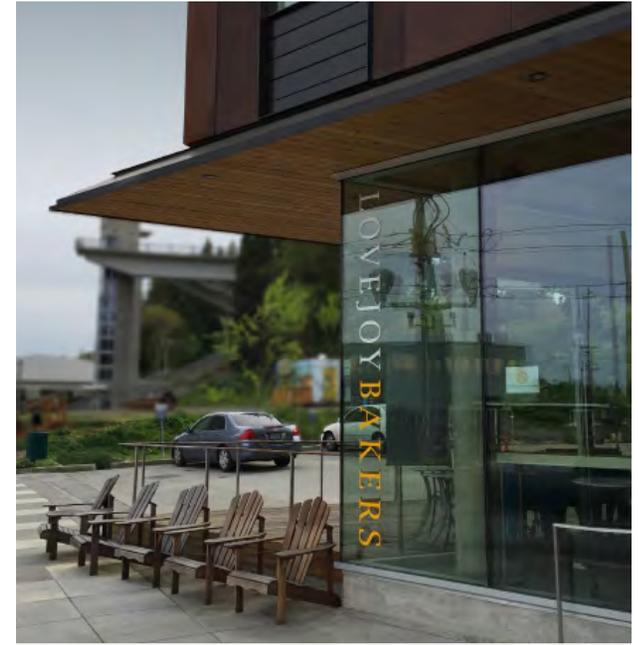
Careful detailing where steel meets glass

Entrance Canopy

Canopies create inviting spaces for the pedestrian. They emphasize human-scaled design and comfort and can act as identifiers for businesses by providing clear orientation to the primary entrance. Canopies can incorporate signage of all scales and can include space for gathering, seating and bike parking.

This guideline may be accomplished by

- » Integration of canopy design with overall building design.
- » Generous protected area both vertically and horizontally, providing good natural light, feeling of openness, and coverage.
- » Incorporation of signage into canopy design.
- » Canopy height should be thoughtfully considered and should be determined by what function the canopy serves. If the primary function of the canopy is to provide weather protection at the entrance or to offer a protected space for café seating, the canopy could be lower in scale to provide a sense of protection. Other types of canopies create outdoor rooms and are larger in scale and, therefore, should be higher to create a sense of openness.



Canopy over storefront provides protected seating



Tall wood structure creates warm inviting space



Glass awning allows natural light to penetrate



Covered areas for pedestrians and bike parking



Multiple levels of canopy for separate use areas



Prominent entrance with outdoor seating



Integrated canopy design with blade sign and seating



Double height canopy with prominent entrance



Signage above and underneath canopy



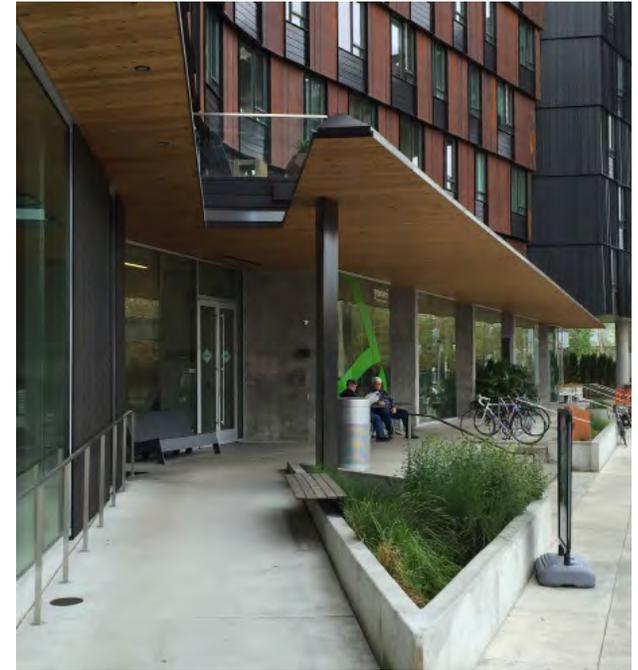
Signage incorporated into canopy

This guideline may be accomplished by

**Clear Orientation,
Inviting and
Human Scaled**

The most beloved urban places mix clarity of structure and form with mystery and adventure and provide inviting, human-scaled places that encourage interaction and exploration.

- » Outdoor rooms accessible from the building.
- » Balance of exterior and interior lighting that makes the building equally inviting day or night.
- » Strong low-slope projecting roof form at building entrance.
- » Bicycle parking adjacent to and visible from the building entrance.
- » Facades are subdivided into smaller, human-scaled elements that are defined by a change of materials.
- » Vertical circulation (stair) is apparent as one approaches the building which encourages active use.
- » Horizontal canopy shields the pedestrian on the sidewalk, defines the commercial realm, and helps to orient visitors to the entries.



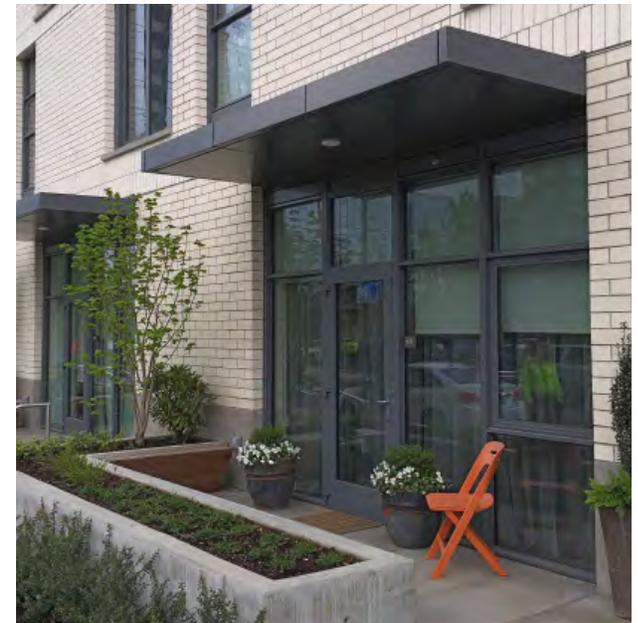
Human-scaled terrace with weather protection



Bike parking and cafe seating adjacent to storefront



Bike parking under partially protected overhang



Live/work space setback from sidewalk

Amendments to LMC 16.24 for Woodland District Design Guidelines
PLANNING COMMISSION HEARING DRAFT
9.20.16

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16.24.040 General standards required for all development.

A. Conflicts. Development within the Woodland District must comply with the standards prescribed in this chapter, Woodland District, and the standards set forth by that document titled “The Pacific Northwest Architectural Style Guidelines and Design Character Elements”. These development standards are intended to implement policies in the adopted Woodland District Strategic Plan. In the event of a conflict between any provision of this chapter, Woodland District, and any other ordinances of the city of Lacey the provisions of this chapter shall prevail with the exception of ordinances whose standards are more restrictive.

B. Master Plan Requirements.

1. Connectivity Master Plan. Connectivity master plans are required for all development within the Woodland District, except lots or parcels exempt from connection spacing standards because of maximum block length requirements; refer to LMC 16.24.050, Streets, through connections and connection spacing.

Development proposals shall show conceptually how the development standards in LMC 16.24.050, Streets, through connections and connection spacing, shall be met in relationship to adjacent property and existing streets, through connections and other paths or trails.

Connectivity master plans shall refer to LMC 16.24.010, regulating plan, subdistricts and streets, and provide review material according to LMC 16.24.120(A), (B) and (C), Submittal requirements.

In addition, the connectivity master plan should generally indicate how open space, parking, driveways, walkways, etc., will relate or connect to adjacent parcels.

Connectivity master plans shall provide review material according to LMC 16.24.120, Submittal requirements. A summary of connectivity master plan eligibility is provided in Table 16.24.040-1.

Future streets and through connections shall be designed and constructed according to Table 16.24.040-2, Improvement Responsibilities, Section B.

2. Detailed Master Plan. Detailed master plans are required for all development in the Master Plan Area where proposed improvements represent fifteen percent or more of the value of the assessed market improvement value of the existing structure with the permit value of the proposed improvement greater than \$20,000.00. For the purposes of determining code compliance, the value of proposed improvements will be based on the value of the building permit for those improvements.

Development proposals shall show conceptually how the development meets the development standards in the following sections:

- a. LMC 16.24.050, Streets, through connections and connection spacing;
- b. LMC 16.24.060, Building, form, siting and site design;
- c. LMC 16.24.070, Building and landscape frontage.

Detailed master plans shall refer to LMC 16.24.010-1, Regulating Plan, Subdistricts; LMC 16.24.050-1, Regulating Plan, Street Types; and LMC 16.24.060-1, Regulating Plan, Building Heights.

Detailed master plans shall provide review material according to LMC 16.24.120, Submittal requirements. A summary of detailed master plan eligibility is provided in Table 16.24.040-1.

Existing built streets shall be improved according to Table 16.24.040-2, Improvement Responsibilities, Section A.

Future streets and through connections shall be designed and constructed according to Table 16.24.040-2, Improvement Responsibilities, Section B.

Master planned streets shall be designed and constructed according to Table 16.24.040-2, Improvement Responsibilities, Section C.

TABLE 16.24.040-1, MASTER PLAN ELIGIBILITY SUMMARY

Eligibility	Connectivity Master Plan	Detailed Master Plan
Lot or parcel is exempt from connectivity standards in LMC 16.24.050, Streets, through connections and connection spacing, due to minimum through block connection spacing.	Exempt	Exempt
Lot or parcel is (1) subject to the connectivity standards in LMC 16.24.050, Streets, through connections, and (2) outside the Master Plan Area, as shown on Figure 16.24.010-1, Regulating Plan, Subdistricts.	Must meet connectivity master plan requirements	Exempt
Lot or parcel is (1) inside the Master Plan Area, as shown on Figure 16.24.010-1, Regulating Plan, Subdistricts, and (2) proposed improvements are less than 15% of the value of the underlying development as defined above or the permit value of the proposed improvement is less than \$20,000.	Must meet connectivity master plan requirements	Exempt
Lot or parcel is (1) inside the Master Plan Area, as shown on Figure 16.24.010-1, Regulating Plan, Subdistricts, and (2) proposed improvements represent 15% or more of the value of the underlying development as defined above with the permit value of the proposed improvement greater than \$20,000.	Must meet detailed master plan requirements	

C. Adjustments. There are three types of adjustments that may be granted by the director, as described below.

1. Proportional Compliance Adjustments. Proportional compliance adjustments apply to lots or parcels fronting on Pacific Avenue and lots or parcels within the Woodland Square Subdistrict and Pacific Subdistrict, as illustrated in Figure 16.24.010-1, Regulating Plan, Subdistricts, and Figure 16.24.050-1, Regulating Plan, Street Types.

Proportional compliance adjustments may be granted by the director to existing development where the value of proposed improvements falls below one of two thresholds. For the purposes of determining compliance, the value of proposed improvements shall be cumulative over the most recent five years, including calculations of all previously exempt remodels, but shall not include life/safety improvements or normal maintenance not requiring a building permit.

- a. When the value of the proposed improvements is less than fifteen percent of the value of the assessed market improvement value of the existing structure, or when the permit value of the proposed improvement is less than \$20,000.00, the applicant may secure a building permit for the proposed improvements without meeting any of the development standards in the LMC.
- b. When the value of the proposed improvements is fifteen percent or greater, but less than seventy-five percent of the value of the existing development, the applicant must meet the development standards of the applicable building or landscape frontage type only. The applicable frontage type standards shall apply to the primary or secondary street-facing side of the block, site or infill block as set out in Table 16.24.050-2, Overview of Streets and Through Connection Types. The designated primary or secondary street-facing edge of the site (or block, or infill block) shall be designed and constructed according to LMC 16.14.070, Building and landscape frontage.
- c. When the value of the proposed improvements is seventy-five percent or greater of the assessed market improvement value of the existing structure, or when the permit value of the proposed

improvement is \$5,000,000.00 or greater, the applicant must meet the development standards of the LMC for the new improvements and the existing building. This \$5,000,000.00 limitation shall be increased on an annual basis in an amount equal to the increase in the Engineering News Record Construction Cost Index from the previous year. Landscaping: the entire site shall meet the applicable development standards. Parking lot reconfiguration and expansion: the entire parking lot shall meet the applicable development standards. External facade modification: the full extent of all the facades shall meet the applicable development standards.

d. Expansion of building footprint: the new square footage associated with the building expansion is required to meet the applicable development standards.

2. Development Standards Flexibility Adjustment. Development standards flexibility adjustments may be granted to any development within the Woodland District, if the director finds that the adjusted development standard will perform as well as the development standard. Eligible development standards and the permitted degree of adjustment is noted in each development standards table.

3. Site Plan Review Committee (SPRC) Adjustment. Any development standards, which are not included in the development standards flexibility adjustment above, or which exceed the permitted degree of flexibility noted in the development standards tables, are eligible for review and approval through the site plan review committee (SPRC), according to LMC 16.24.090, Form-based code review.

D. New and Existing Streets and Through Connections. Development standards of this chapter are intended to establish a complete network of new and existing streets and through connections, which may take the form of local streets, multi-use paths or woonerfs. The location of new and existing streets and through connections and their required intersections are mapped in Figure 16.24.050-1, Regulating Plan, Street Types. Connection types permitted and maximum spacing of new and existing streets and through connections are established in Table 16.24.050-1, Connections and Connection Spacing. The required improvements for each street and through connection, including the sidewalk zones, are specified in Figures 16.24.050-2 through 16.24.050-9. When the property owner or developer is responsible for dedication of land and/or specific constructed improvements it is noted in Figures 16.24.050-2 through 16.24.050-9. Table 16.24.040-2 summarizes the improvements and the responsibility of each party, whether city or property owner/developer.

TABLE 16.24.040-2, IMPROVEMENT RESPONSIBILITIES

Summary of Requirements	Eligible Streets
<p>A. Existing Built Streets. Existing streets are required to meet requirements for sidewalk improvements, street lights, street furnishings, and trees, according to Table 16.24.050-1, Table 16.24.050-2 and Figures 16.24.050-2 through 16.24.050-9, street types and sidewalk improvements. Improvements and, where noted in the development standards, dedication of land are the responsibility of the property owner/development applicant.</p>	<p>3rd Avenue SE 6th Avenue SE 7th Avenue SE Pacific Avenue SE Sleater Kinney Road SE College Street SE</p>
<p>B. Future Streets and Through Connections. Right-of-way is dedicated by property owner/development applicant. The city constructs the street to city standards as established by Table 16.24.050-1, Table 16.24.050-2 and Figures 16.24.050-2 through 16.24.050-9, street types. An interim bike-pedestrian trail may be required on the dedicated right-of-way, prior to completion of the street improvements. Any interim bike or pedestrian trail shall be designed and constructed in compliance with through connection development standards in Figure 16.24.050-8 and Table 16.24.050-9, and shall meet, at a minimum, the development standards and minimum requirements (set out in Figure 16.24.050-8, Through Connection, Minimum Requirements, and Table 16.24.050-9, Through Connection, Minimum Requirements and Optional Components).</p>	<p>Unbuilt 4th Avenue SE Unbuilt segment of Golf Club Road SE Unbuilt segment of 10th Avenue SE All through connections and other streets outside of the Master Plan Subdistrict</p>
<p>C. Master Planned Streets. Master planned streets shall be located and constructed according to an approved master plan that meets the requirements of a connectivity master</p>	<p>All other streets and through connections within the master plan</p>

Summary of Requirements	Eligible Streets
plan or a detailed master plan, as applicable. The property owner or developer is responsible for all required street or through connection improvements, according to Tables 16.24.050-1 and 16.24.050-2 and Figures 16.24.050-2 through 16.24.050-9, street types and sidewalk improvements.	subdistrict

E. Environmental Performance.

1. It shall be the responsibility of the operator and/or the proprietor of any proposed use to provide such evidence and technical data as the director and/or site plan review committee may require to demonstrate that the use or activity is or will be in compliance with the environmental performance standards of Chapter 16.57 LMC.
2. Failure of the director and/or site plan review committee to require such information shall not be construed as relieving the operator and/or the proprietor from compliance with Chapter 16.57 LMC, Environmental Performance Standards.
3. All stormwater runoff shall be retained and disposed of on site or disposed of in a system designed for such runoff and which does not flood or damage adjacent properties. Systems designed for runoff retention and control shall comply with specifications provided by the city and shall be subject to its review and approval, and shall, moreover, comply with Chapter 15.22 LMC pertaining to community facilities.

TABLE 16.24.040-3, DISTRICT WIDE DEVELOPMENT STANDARDS

Section/Description	Urban Neighborhood 1 Woodland Square	Urban Neighborhood 2 Pacific Avenue	Urban Neighborhood 3 Master Plan Area
Stormwater	All projects shall meet the amended 2010 City of Lacey Stormwater Design Manual as hereafter amended, which has square footage thresholds for development and redevelopment (2,000 square feet and 5,000 square feet of new or redeveloped impervious surfaces). No requirement for areas under 2,000 square feet; between 2,000 and 5,000 square feet the stormwater must be retained on site; and over 5,000 square feet full treatment and infiltration is required.		
Tree Preservation	<p>Tree protection professional report required. A qualified professional forester shall review the site and provide a report analyzing the site for tree protection consistent with the requirements of this chapter. The report shall provide information important to urban forest management and options for consideration when developing preliminary designs. The report shall suggest options for design to best achieve the purposes of the Urban Forest Management Plan and this chapter. The report shall include but shall not be limited to:</p> <ol style="list-style-type: none"> a. An analysis of technical information requested by the review body related to trees and forest practices; b. Analysis of what portion of the site is best for designation of the tree tract if required, considering the intent of this chapter, soil type, topography, tree species, health of trees and reasonable project design limitations; c. Recommendations for saving of individual tree specimens based upon the intent of this chapter, soil type, topography, tree species, health of trees, and reasonable project design limitations; d. A plan for protection of trees to be saved during construction including placement of construction fences, monitoring of construction activity and other measures necessary to ensure adequate tree protection; e. Consideration of the location of roads, other infrastructure, and buildings and potential options for alternative locations, if applicable, to best satisfy the purposes of the Urban Forest Management Plan; f. A timeline for tree protection activity; and 		
	<ol style="list-style-type: none"> g. The final tree protection plan should be prepared on the site grading plan. All tree protection fences, trees to be saved, and trees to be removed should also be shown on the site demolition plan. Necessary save tree pruning and selective thinning within tree tracts shall be detailed and trees marked as such. The tree protection plan and demolition plan should be part of the submittal to the city of Lacey and shall be approved by the tree protection professional. The tree protection plan shall be part of the contractor bid package and a copy of the tree protection plan shall be available to the contractors 		

Section/Description	Urban Neighborhood 1 Woodland Square	Urban Neighborhood 2 Pacific Avenue	Urban Neighborhood 3 Master Plan Area
	<p>on site at all times during logging, clearing, and construction. Development standards may be adjusted on a case-by-case basis to protect priority trees: Priority tree types: Trees to be protected must be healthy, wind firm, and appropriate to the site at their mature size, as identified by a qualified professional forester. In designing a development project, the applicant shall protect the following types of trees in designated tract(s) in the following order of priority:</p> <ol style="list-style-type: none"> 1. Historical trees. Trees designated as historical trees under LMC 14.32.072. 2. Specimen trees. Unusual, rare, or high quality trees. 3. Critical area buffer. Trees located adjacent to critical area buffers. 4. Significant wildlife habitat. Trees located within or buffering significant wildlife habitat. 5. Other high quality individual trees or groves of trees. 		

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Ryan Andrews

From: Zach Kosturos <zach@primelocations.com>
Sent: Friday, September 09, 2016 8:35 AM
To: Ryan Andrews
Cc: Rick Walk; Joni Baker; Priscilla Terry
Subject: Re: Notice of Public Hearing: Woodland District Style Guidelines

Ryan,

I think not allowing Stucco or metal as a main exterior material is a mistake. There are some fabulous buildings that use both of those.

Also, I never understand how we get these grand ideas of making our buildings look like the ones you see in major metro areas? We don't get anywhere CLOSE to the rents they do and consequently, can't build for the cost they do.

I think the vision is nice to have to hold firm to those standards will likely delay re-development.

After all, the Woodland District has less traffic through it than many other areas in Lacey and traffic and density drive mixed use development.

Just my two cents....



Zach Kosturos

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On Fri, Sep 9, 2016 at 8:30 AM, Ryan Andrews <Randrews@ci.lacey.wa.us> wrote:

PUBLIC HEARING NOTICE

Notice is hereby given that the City of Lacey Planning Commission will conduct a public hearing on Tuesday, September 20, 2016, beginning at 7:05 p.m. at Lacey City Hall, 420 College St. S.E., in the Council Chambers, for consideration of the following item:

The proposed adoption of the Woodland District Style Guidelines (see attached) developed to supplement the architectural requirements of the Woodland District Form-Based Code. The Guidelines provide details for building material palette, glazing, entrance canopies, and other details that will articulate the vision for the district. Although the guidelines will not be codified in the Lacey Municipal Code, they will apply to development projects as regulations and will be adopted by the City Council.

The above summary is only an overview of the proposal. For a complete review of the proposal, interested individuals should contact the Community Development Department for a review draft of the proposed amendments.

The purpose of the public hearing is to give citizens an opportunity to testify to the Planning Commission regarding the above proposals. Any interested citizen may testify. Anyone who cannot attend the meeting may give testimony in a letter addressed to: Lacey Planning Commission, 420 College St. SE, Lacey, WA 98503. If your letter is received before the hearing, it will become part of the public record. Prior to the hearing, information regarding the proposal, including an environmental impact analysis, may be obtained from the Lacey Community Development Department at City Hall; or you may phone (360) 491-5642.

If you need special accommodations to participate in this meeting, please call us at (360) 491-5642 by 10:00 a.m. the day before the meeting.

Ryan Andrews, Planning Manager

City of Lacey Community Development

420 College St. SE

Lacey, WA 98503

(360) 412-3190

randrews@ci.lacey.wa.us



LAND USE COMMITTEE
October 3, 2016

SUBJECT: 2016 Development Code Housekeeping Amendments

RECOMMENDATION: The Committee will be briefed on the Planning Commission Recommendation. No action is needed. The recommendation will be schedule for consideration by the City Council at a public meeting.

STAFF CONTACT: Scott Spence, City Manager *SS*
Rick Walk, Community Development Director *RW*
Ryan Andrews, Planning Manager *RA*

ORIGINATED BY: Community Development Department

ATTACHMENTS: 1. Draft Ordinance

FISCAL NOTE: None.

PRIOR REVIEW: The 2016 Development Code Amendments were adopted by the City Council on August 11, 2016.

BACKGROUND:

On August 11, 2016 the City Council adopted the update to the City's Comprehensive Plan and the 2016 slate of amendments to the City's Development Code. In codifying the amendments, Code Publishing (the company that maintains our online municipal code) made staff aware of some slight modifications needed to other sections of the code when the Business Park zone was changed to the new "Community Office" zone.

The code changes recommended by Code Publishing include:

- 14.23.083(D)(12): change "business park" to "community office"
- 16.70.050(A)(3): change "Business park zone" to "Community office zone"
- 16.72.030(D), (D)(1)(a): change business park references in these sections to community office

Even though these changes are not substantive but are scrivener's errors, they do need to be corrected through the standard code amendment process. The Planning Commission recommends that the City Council adopt the ordinance correcting identified scrivener errors.

ADVANTAGES:

1. Adopting the housekeeping amendments will eliminate inconsistencies in language between the previous Business Park zone and the new Community Office zone.

DISADVANTAGES:

1. None identified.

ORDINANCE NO. _____

CITY OF LACEY

AN ORDINANCE OF THE CITY OF LACEY MAKING CERTAIN HOUSEKEEPING CHANGES TO THE LACEY MUNICIPAL CODE, AMENDING SECTIONS 14.23.083, 16.70.050, AND 16.72.030, ALL OF THE LACEY MUNICIPAL CODE, AND APPROVING A SUMMARY FOR PUBLICATION.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY COUNCIL OF THE CITY OF LACEY, WASHINGTON, as follows:

Section 1. Section 14.23.083 of the Lacey Municipal Code is hereby amended to read as follows:

14.23.083 Industrial Design Guidelines

A. Site organization for multiple building complexes

1. Placement and orientation of multiple structures shall be designed to provide configurations that do not result in a strict geometric or monotonous look.
2. Multiple building placement should create the impression that the structures within the complex were planned and designed to compliment one another. However, each structure shall be of independent use and function except as provided for structures with shared facilities as set forth below.
3. Generally buildings shall be separated a distance similar to what would be expected if located on separate lots (60 feet or more considering setbacks, parking and/or access and yard area).
4. Structures may be arranged in a configuration to share common loading, unloading and truck maneuvering facilities for efficiency, reduction of impervious surfaces and general functionality.
5. Site components such as structures, parking, driveways, and out-door functions shall be arranged and located to emphasize the aesthetically pleasant components of the site such as existing mature trees and views, or superior architectural features, and disguise its less attractive scenes such as service facilities, outside storage and equipment areas, and trash enclosures through placement and design of the structure and landscaping.

B. General site circulation for multiple building projects.

1. Circulation shall be designed for the required function of moving trucks in an efficient fashion, but also to provide an attractive and natural flowing network.
2. Design, construction, and the look of access ways/streets throughout the parcel shall be similar to the result which would occur by a subdivision of the property into separate lots, including a city street system with collectors, local access ways, and private drives.
3. As much as practical and avoiding a loss of function, effort shall be taken to design an interconnected access system that avoids a strict geometric grid and provides interest. Long straight streets that transect a whole parcel should be avoided for light curves that provide interest and bring street trees and buildings into the natural line of site.

C. Additional site design guidelines.

1. For those industrial uses that utilize outdoor storage area, inventory shall be placed in an orderly manner and shall be screened from right of way areas.
2. Site features such as fences, walls, refuse and recycle enclosures, and light fixtures shall be designed to be consistent with the scale and aesthetic character of the building.
3. Landscaping shall meet requirements of Chapter 16.80 LMC for the intended function. Particular emphasis shall be focused on buffering and compatibility issues when adjacent to a zoning classification with different land use intensity.
4. In multi-building complexes, internal landscape areas shall be developed and designed in a similar fashion as if individual lots were developed and yard areas landscaped. Landscaping design should provide a visual connection between buildings and take maximum advantage of landscaping by providing amenities for workers such as outdoor lunch areas and plazas between selected buildings.
5. All applicable requirements of LMC 14.23.084 and 14.23.086 shall apply.
6. All signs shall meet standards of Chapter 16.75 LMC.
7. Site lighting.
 - a. Lighting shall be installed throughout the site for entryways, parking lots, and pedestrian areas. This shall be achieved both at a pedestrian and overall site level.

- b. Lighting shall be activated by photo electric cells or timers.
- c. Lighting fixtures shall complement project design and shall be oriented to avoid direct glare to adjacent properties, particularly where properties are located in a different zoning classification.

8. Refuse:

- a. Refuse container screening shall be required and be of a material and design compatible with the overall architectural theme of the associated structure, shall be at least as high as the refuse container, and shall in no case be less than six feet high.
- b. No refuse container shall be permitted between a street and the front of the building.
- c. Refuse collection areas shall be designed to contain all refuse generated on site and deposited between collections. Deposited refuse shall not be visible from outside the refuse container.

D. Architecture and building design

- 1. New buildings shall maintain diversity and individuality in style while improving the aesthetic character of their surrounding area.
- 2. In multi-building complexes, a comprehensive architectural concept shall be developed and maintained. Various site components should be unified through the use of similar design, material and colors.
- 3. New buildings shall have three distinct components: base; middle; and top. Each component shall be defined by horizontal and/or vertical articulation. Facade articulation may consist of changes in the wall plane, use of openings and projections, and material and color variations. Exceptions may be permitted by the Site Plan Review Committee only where a specific architectural style offers other types of building form and facade articulation.
- 4. Where function necessitates a basic, box-like building form, design strategies shall be utilized to provide an attractive facade where portions of the structure are visible to the public. A preferred technique is to provide exterior articulation such as change in color, material, or plane introduced on an outer decorative shell. Other architectural strategies

may also be used, provided they accomplish the same purpose to provide a more attractive facade.

5. Less architecturally interesting facades of buildings shall be substantially setback and screened from public view by mature, dense landscaping. Landscaping size and location shall be determined on a site-by-site basis and may exceed the minimum required in the City Code.

6. To avoid flat walls building modulation shall be used to reduce the mass and bulk of the structure. This can be achieved by utilizing the following techniques:

- a. Building setbacks on upper floor levels;
- b. Recessed or clearly defined entryways;
- c. Building ornamentation;
- d. Varying roof lines, pitches, and shapes;
- e. Overhangs, awnings and marquees;
- f. Dormers, balconies, porches, staircases;
- g. Window and door fenestration.
- h. Architectural technique that will have a modulation effect and meet the intent of this section.
- i. Landscaping trellises with climbing vegetation or art work may be utilized for building walls that are located within public viewing.
- j. Use of glass and storefront window systems;
- k. Use of reveals to create patterning in the concrete
- l. Main entrances to the buildings shall be well defined.

7. New buildings shall have at least one major focal point and minor focal points. Focal points may be achieved through horizontal and vertical lines, change in material, change in color, change in the form and shape of a portion of the building, or other means. Combining the main entrances and the focal points is encouraged.

8. Service doors visible from public streets shall be recessed and integrated into the overall design of the building, or have some other architectural treatment to enhance its appearance.

9. Buildings located within a multiple building complex shall utilize similar or complimentary colors, materials, window patterns and roof forms.

10. Consideration shall be given to colors and materials. Each shall complement the other and enhance existing buildings.

11. Consistency shall be used with architectural elements to ensure that building walls that are highly visible to the public are compatible with one another.

12. Building design for industrial uses adjacent to ~~business park~~community office, commercial, open space institutional or residential zone districts shall blend with the adjacent neighborhood and create an attractive and functional transition between the zones. Items to consider include such things as scale, building height, colors, and materials to promote the blending of the zones and setbacks and buffering to reduce impacts from incompatible elements of the different zones and provide an appropriate transition.

E. Compatibility with adjacent residential and open space institutional developments.

1. Where industrial buildings are to be located adjacent to different zone districts, care shall be taken to ensure compatibility and integration of the development with the adjacent area. This will include the physical layout of the site plan, the architectural design of the buildings and improvements, and consideration of mitigation techniques to reduce or eliminate identified impacts to the adjacent environment. Impacts such as, but not limited to, light/glare, noise, traffic and aesthetics shall be considered.

2. In development of the site plan, consideration shall be given to how the layout fits into the surrounding neighborhood.

3. Architectural design shall consider compatibility with the character and style of adjacent uses. Special techniques shall be considered to adequately address impacts, such as security and automobile light and noise issues through such techniques as landscaping, buffering and other measures.

Section 2. Section 16.70.050 of the Lacey Municipal Code is hereby amended to read as follows:

16.70.050 General location standards.

A. Retail stands may only be located in the following zones:

1. All commercial zones.
2. All light industrial zones.
3. ~~Business park~~Community Office zone.
4. Mixed high density corridor and mixed moderate density corridor.
5. Along arterials and collectors at key multimodal intersections in pocket parks or pedestrian plazas as approved by the approving authority based upon findings that it will enhance the local pedestrian experience.
6. Open space institutional zones.

B. Retail stand locations shall be compatible with the pedestrian and the vehicular nature of the zone, the use of the right-of-way as a public thoroughfare, the use of parking lots as public parking areas, and/or the use of an open air plaza. The site shall be located to enhance the pedestrian nature of the zone and shall not be located so as to attract or serve vehicular traffic.

C. Temporary/seasonal retail stands may be located to attract and serve vehicle traffic.

D. In determining whether or not the proposed location would be permitted, the following criteria shall be considered:

1. The type and intensity of the proposed use and the type and intensity of existing uses;
2. The width of the sidewalk, pedestrian plaza or parking lot in which it is to be located;
3. The proximity and location of existing street furniture, including but not limited to signposts, lampposts, bus shelters, benches, phone booths, trees, newsstands, as well as the presence of bus stops and truck loading areas;
4. Established or proposed pedestrian and vehicular traffic patterns;

5. The number of available retail stand sites in a given area or zone of the city and the number of existing retail stands in such area;
 6. Other factors deemed relevant by the approving authority, consistent with the purpose of this chapter and intent of the zone proposed for the use.
- E. The retail stand and location shall promote the diversity of retail stand activity;
 - F. The site and retail stand together shall not create a pedestrian or vehicular traffic hazard;
 - G. The retail stand shall be compatible with uses in the general vicinity and adjacent properties;
 - H. The retail stand location shall promote the pedestrian nature of the general area in which it is located;
 - I. The retail stand location shall be compatible with the public interest in the use of the sidewalk as a public right-of-way and the use of a public or private parking lot for the primary intended use of vehicular parking and, as such, shall not endanger the public health, safety and welfare.

Section 3. Section 16.72.030 of the Lacey Municipal Code is hereby amended to read as follows:

16.72.030 District parking requirements.

- A. The requirements for any use not listed herein shall be those of the listed use most similar to the unlisted use. When similarity is not apparent, the enforcing officer and/or the site plan review committee shall determine the minimum and maximum for the unlisted use.
- B. For conditional uses, as identified and described in Chapter 16.66 LMC, the parking requirement shall be as provided in that chapter or as determined by the site plan review committee.
- C. Residential District. Off-street parking requirements for residential districts are located in Table 16T-13.
- D. Commercial, ~~Business Park~~Community Office and Industrial Uses.
 1. General Parking Standards--Parking Standard Table.

a. In the several commercial, industrial, ~~business park~~community office, and mixed use districts, off-street parking requirements shall be as shown in Table 16T-13; provided, that all of the property is controlled by a single person or corporation, or written agreements for shared parking, acceptable to the city, are filed with the enforcing officer.

b. **Phased Reduction of Maximum Parking Standards.** One technique for transportation demand management (TDM) is to reduce maximum allowable parking spaces. This can be done by slowly phasing down the maximum allowable number of parking spaces over a period of years. This technique has advantages of reducing vehicle trips and conserving urban commercial land that can be used for other purposes. However, this technique has the potential to have a significant adverse impact on the jurisdiction's economic development if other reasonable forms of alternative transportation are not available. This technique should be periodically revisited to consider its viability but should not be implemented until its viability for Lacey is established.

c. **Minimum Optional Guidelines and Maximum Standards.** To promote parking reduction, the optional minimum guideline serves as a suggested parking number but is not mandatory for automobiles except for single-family residential development. Applicants will be encouraged to provide less automobile parking than the minimum listed whenever possible based upon TDM available on-street parking, potential shared parking within walking distance and other factors. The minimum number listed for bicycle parking shall be provided as indicated in the table for both commercial and residential development.

The number of parking spaces needed to serve a project must be demonstrated to the site plan review committee based upon a parking plan considering TDM techniques and other relevant factors. Upon justification to the satisfaction of the committee, whatever number of parking spaces agreed upon shall be the number required, and this shall be an enforceable condition of the approval.

The maximum parking standards may be increased if the site plan review committee finds compelling reasons to do so. Such determination shall be at the sole discretion of the committee based upon such factors as unique site or use requirements, historical data of a particular use or other relevant factors indicating additional parking is necessary to properly serve a use or uses at a site.

Shared parking agreements are encouraged if the physical relationship between the premises is within convenient walking distance and makes such sharing possible.

2. If more parking spaces than the maximum permitted by Table 16T-13 exist on the subject property, an owner/developer may lease those excess spaces until conformance with Table 16T-13 is reached.

a. Shared parking is particularly encouraged when:

(1) The total number of parking spaces provided is sufficient to meet expected demand; or

(2) The uses are operating during different hours and number of spaces needed for all uses at any one time are satisfied. If permitted, the total number of spaces for a shared arrangement should be no greater than the maximum number of spaces permitted for all uses operating at the same time.

b. Where adjoining parking facilities of two or more ownerships are developed and designed as one parking facility, a reduction of required parking spaces may be permitted based upon the increased potential opportunity for shared parking and other parking reduction techniques.

c. The continuation of joint or shared facilities should be assured by a sufficient legal document such as a covenant or reciprocal easement agreement or by participation in a local improvement district or parking association. Joint or shared parking associated with multi-tenant retail and commercial uses will be considered to be a shared parking facility. Lease agreements will satisfy the requirement for a sufficient legal document. However, any new tenant whose parking requirement reduces the total parking available in the shared parking facility below seventy-five percent of the requirements for all uses sharing the facility will be required to provide additional parking.

d. For large projects where a traffic study is required under Chapter 14.21 LMC (Traffic Mitigation and Concurrency) and the proposal has one hundred or more employees, a comprehensive TDM strategy may be proposed to achieve a reduction in minimum parking listed in the parking table. Additionally, a bonus credit towards transportation impact fees may be awarded for reducing parking area pursuant to this section. The reduction in parking permitted and the incentive bonus shall be

commensurate with the permanence, effectiveness and demonstrated reduction in off-street parking demand effectuated by such alternative programs.

Alternative programs that may be considered by the enforcing officer and/or site plan review committee under this provision include, but are not limited to, the following:

- (1) Private vanpool operation;
- (2) Transit/vanpool fare subsidy;
- (3) Imposition of a charge for parking;
- (4) Provision of subscription bus services;
- (5) Flexible work hour schedule;
- (6) Capital improvements for transit services;
- (7) Preferential parking for carpools/vanpools;
- (8) Participation in the ride-matching program;
- (9) Reduction of parking fees for carpools and vanpools;
- (10) Establishment of a transportation coordinator position to implement carpool, vanpool and transit programs;
- (11) Bicycle parking facilities including associated shower and changing facilities;
- (12) Compressed work week;
- (13) Telecommuting;
- (14) Other techniques and strategies approved by the Site Plan Review Committee.

e. Parking reduction under this subsection (D)(2) must provide information regarding the administration of the program to the site plan review committee. The information must include:

- (1) Address each individual TDM strategy as part of the transportation impact analysis;
- (2) Provide the city with an estimate of peak hour employees as part of their site plan review application and traffic impact analysis;
- (3) Provide estimated parking occupancy rates for the development as part of the transportation impact analysis showing average weekday use;
- (4) Demonstrate how TDM strategies will be used to minimize the need for parking.

3. The city is hereby authorized to develop parking requirements specific to certain geographic locations or districts rather than individual sites by pooling parking supply in desired locations. Any parking requirements established as part of these programs may deviate from the typical standards contained herein and would be binding on the properties within these locations.

E. Transportation Demand Management Incentives. The site plan review committee shall determine whether the applicant can receive a traffic impact mitigation fee reduction for implementation of TDM strategies that reduce parking needs. Such incentives will be as listed in the following schedule as well as any traffic mitigation credit allowed under LMC 14.21.030. The incentive bonus is as follows:

1. Any developer who builds at the minimum amount of parking allowed shall receive a five percent trip reduction in the calculation of traffic impacts.
2. Any developer who builds at or below the minimum and includes significant strategies from subsection (D)(2)(d) of this section is eligible for an additional five percent trip reduction bonus.
3. Additional bonuses will be allowed under the provisions of LMC 14.21.030.

¹ When calculating need, a lower ratio of five-tenths per one thousand square feet of GFA can be justified when a covenant is attached to the property that limits the occupancy load to ninety-five percent of the parking stalls available. In addition, the SPRC may authorize a parking ratio up to five spaces per one thousand square feet of GFA if the need can be demonstrated.

² If retail space in a mixed-use development exceeds forty percent of the gross floor area of the development, the retail use parking requirements of this section apply to the entire space.

³ Gross square feet does not include enclosed or covered areas used for off-street parking or loading, mechanical floor areas or covered public spaces.

⁴ School and/or public facility parking spaces may be used provided the facilities are on the same or contiguous parcels within three hundred feet of the theater or auditorium.

⁵ In mobile home parks, the parking spaces in excess of one per mobile home may be grouped in shared parking areas.

⁶ For single-family residential development, a minimum of two parking spaces is required.

The following notes apply to all of the above uses:

- Minimum automobile spaces listed in the table are optional guidelines provided in LMC 16.72.030(D)(1)(c) (optional minimum guidelines). Minimum parking spaces for bike parking are mandatory.
- Parking ratios for mixed use development projects shall be determined by calculating the percentage of GFA by use multiplied by the appropriate parking ratio for each use plus a five percent parking reduction for two uses, ten percent parking reduction for three uses and fifteen percent parking reduction for four or more uses.
- Parking spaces provided as part of the above/below grade parking amenity identified in Table 14T-12 shall be exempt from all maximum parking requirements.
- All major employers or major worksites, as defined by RCW 70.94.524, shall designate at least five percent of auto spaces as carpool spaces. These spaces must be located as close to the main employee entrance as possible and shall be called out on the site plan.
- Where adjacent roads are designed for on-street parking and approved by the public works director, parking credit may be given for on-street parking.

Section 4. SEVERABILITY. If any section, sentence, clause, or phrase of this ordinance should be held to be invalid by a court of competent jurisdiction, such invalidity or unconstitutionality shall not affect the validity or constitutionality of any other section, sentence, clause, or phrase of this ordinance.

Section 5. CORRECTIONS. The City Clerk and the codifiers of this ordinance are authorized to make corrections to this ordinance including, but not limited to, the corrections of scrivener's/clerical errors, references, ordinance numbering, section/subsection number and any references thereto.

Section 6. The Summary attached hereto is hereby approved for publication.

PASSED BY THE CITY COUNCIL OF THE CITY OF LACEY, WASHINGTON, at a regularly-called meeting thereof, held this ____ day of _____, 2016.

CITY COUNCIL

By: _____

Mayor

Approved as to form:

City Attorney

Attest:

City Clerk

SUMMARY FOR PUBLICATION

ORDINANCE _____

CITY OF LACEY

The City Council of the City of Lacey, Washington, passed on _____, Ordinance No. _____, entitled "AN ORDINANCE OF THE CITY OF LACEY MAKING CERTAIN HOUSEKEEPING CHANGES TO THE LACEY MUNICIPAL CODE, AMENDING SECTIONS 14.23.083, 16.70.050, AND 16.72.030, ALL OF THE LACEY MUNICIPAL CODE, AND APPROVING A SUMMARY FOR PUBLICATION."

The main points of the Ordinance are described as follows:

1. The Ordinance updates certain sections of the Lacey Municipal Code, changing the term "business park" to "community office."
2. The Ordinance approves this summary for publication.

A copy of the full text of this Ordinance will be mailed without charge to any person requesting the same from the City of Lacey.

Published: _____, 2016.