



**LACEY CITY COUNCIL
WORKSESSION
THURSDAY, JANUARY 7, 2016
7:00 – 9:00 P.M.
LACEY CITY HALL – 420 COLLEGE STREET SE**

AGENDA

- 7:00 HOME CONSORTIUM AND CIP STATUS UPDATE**
*GARY ADEN, THURSTON COUNTY HOUSING AND COMMUNITY RENEWAL
PROGRAM MANAGER*
(PRESENTATION – NO ATTACHMENT)
- 7:45 WOODLAND DISTRICT FORM-BASED CODE UPDATE**
RYAN ANDREWS, PLANNING MANAGER
(STAFF REPORT ATTACHED)
- 8:45 TRANSPORTATION BENEFIT DISTRICT STATUS UPDATE**
SCOTT EGGER, PUBLIC WORKS DIRECTOR
(VERBAL – NO ATTACHMENT)
- 9:00 ADJOURN**



LACEY CITY COUNCIL WORKSESSION January 7, 2016

SUBJECT: Woodland District Hybrid Form-Based Code

RECOMMENDATION: Conduct a work session with the City's consultant team on the Woodland District Hybrid Form-Based Code. The Planning Commission recommendation will be considered by the full Council at an upcoming regular meeting.

STAFF CONTACT: Scott Spence, City Manager *SS*
Rick Walk, Community Development Director *RW*
Ryan Andrews, Planning Manager *RA*

ORIGINATED BY: Community Development Department

ATTACHMENTS: 1. [Draft Woodland District Hybrid Form-Based Code](#)

FISCAL NOTE: None.

PRIOR REVIEW: The City Council attended the culminating presentation for the weeklong charrette prior to the City Council meeting on March 26, 2015. The Land Use Committee was briefed on November 2, 2015.

BACKGROUND:

The Woodland District Strategic Plan, which was adopted by the Lacey City Council on July 25, 2013, set forth short and long-term strategies to enhance the District as a place to gather, interact, live, shop and play. The primary action in the "Set the Stage" portion of the action plan developed for the Woodland District was to develop a form-based code. The "Set the Stage" section is related to actions that can be taken to catalyze development and are designed to provide a foundation for investment into the District.

The form-based code is unique in that it uses both graphics and a concise narrative to clearly illustrate the District's desired form and character. Unlike standard zoning codes, the form-based code shapes both the public realm (streets and pedestrian network, public places, and on-street parking) and private realm (building forms, design and locations, on-

site parking, and landscaping). The main benefit of the form-based approach is that it provides clarity of content and permitting process which is of benefit to potential developers. The code would ultimately serve as an incentive to attract new investments and produce a cohesive neighborhood characterized by great places.

With the 2015 budget, the City Council authorized \$100,000 for consultant services related to the development of the form-based code. The City has hired SCJ Alliance to lead the project with support from Urbsworks, a Portland-based design firm and Community Attributes, a Seattle-based economics consultant. To assist the City and the design team, the City established a technical review team made up of various stakeholders including architects, landscape architects, property owners, brokers, etc. to act as the steering committee through the process. The technical review team conducted five meetings throughout the process to develop ideas, review key concepts, examine market conditions, and test the code.

Much of the work to develop the form-based code occurred at a week-long charrette. The charrette was held the week of March 23rd through March 26th and was a series of hands-on workshops, open houses, drop-in discussions with the project team, and culminated in a final public presentation.

The final draft code has been developed consistent with the Woodland District Strategic Plan and integrated into the City's existing regulatory framework to ensure high-quality public spaces defined by a variety of building types and uses including housing, retail, mixed-use and office space. The new code will incorporate a regulating plan, building form standards, street standards (plan and section), general use regulations, and other elements needed to implement the vision for the Woodland District.

The Planning Commission conducted a public hearing on the Form-Based Code on October 6th. Notice of the hearing was mailed directly to all property and business owners within the district, notice was also provided to all attendees of previous Woodland District events, and social media was utilized to notify and solicit input. No written or verbal testimony was received. At the conclusion of the public hearing, the Planning Commission recommended adoption of the form-based code subject to re-examination of the proportional compliance procedures.

As recommended by the Planning Commission, the consultant team has analyzed the proportional compliance requirements (section 16.24.030 beginning on page 7 of the attached draft). These requirements are essentially trigger points that dictate whether partial or full compliance with the form-based code is required when dealing with property redevelopment. Since most of the property in the Woodland District is developed and most new projects will be redevelopment projects, it is very important that the thresholds be set appropriately. At the work session, the consultant team and staff will review the methodology with the City Council and review the proposed thresholds to ensure that they are set at appropriate levels to achieve the desired compliance outcomes.

Prior to adoption, the consultant team will be conducting staff training so that staff can familiarize themselves and become fluent in the methodology associated with a new code. The consultant team will also be developing informational and promotional materials that will be used for potential applicants and property owners who are considering projects under the new code. It is anticipated that the final draft form-based code will be brought to the full council for adoption in February.

ADVANTAGES:

1. The Woodland District Hybrid Form-Based Code will implement the Woodland District Strategic Plan by setting forth and clearly illustrating the District's desired form in a concise set of regulations.
2. The Code contains clear content, graphics, and a concise narrative to make the permitting process more predictable and ultimately catalyzing private development in the District.

DISADVANTAGES:

1. None identified.

16.24 Woodland District Hybrid Form Based Code

Final Draft 22 September 2015

16.24.010 Statement of Intent

It is the intent of this Chapter to:

- A. Encourage density and a diverse mix of uses in the core area;
- B. Create a core area that is strongly pedestrian-oriented and transit friendly;
- C. Create a strong identity for the core area that supports the policies of the Downtown Comprehensive Plan, the Downtown 2000 Plan, and the Woodland District Strategic Plan;
- D. Create places that provide for the needs of a diverse population;
- E. Provide a comfortable pedestrian experience and commercial-retail opportunities;
- F. Promote the development of an office hub within the Woodland District that supports the surrounding retail component; and
- G. Promote high density residential in mixed-use patterns throughout the Woodland District.

Woodland District Urban Neighborhoods Intent Statements

The Woodland District is divided into three sub-districts:

Urban Neighborhood 1 – Woodland Square

The goals for Urban Neighborhood 1 promote development that strives to combine commercial and housing uses on a single site or in close proximity. The Form Based Code (FBC) allows increased development on busier streets without fostering a strip commercial appearance. Development will support transit use and provide new housing opportunities in the City. The emphasis of the commercial uses is primarily on commercial, service, medical, educational, office, and locally-serving retail. Development is intended to consist primarily of businesses on the ground floor with services and/ or housing on upper stories. Development is intended to be pedestrian-oriented with buildings close to and oriented to the sidewalk, especially at corners, with through block connections that provide connections between properties, and an overall modified grid of streets.

Urban Neighborhood 2 – Pacific Avenue

The goals for Urban Neighborhood 2 promote low- and medium-intensity development with a local or regional emphasis along Pacific Avenue SE. The zone is intended to prevent the appearance of strip commercial development. Development is expected to balance the needs of pedestrians, bicycles, automobiles, and transit with a mixture of commercial, service, and residential uses. Although Pacific Avenue is the primary street for lots and parcels, Infill Blocks that have multiple frontages where the site (parcel) abuts the Woodland Trail development may use the trail as a ground level street-facing elevation of buildings and will take advantage of pedestrian connection to the trail. Development will balance the needs of autos, pedestrians, and transit users.

Urban Neighborhood 3 – Master Plan Areas

The goals for Urban Neighborhood 3 promote development that strives to combine regional commercial retail and housing uses on a single site or in close proximity. The development standards allow increased development on busier streets without fostering a strip commercial appearance. Development will support transit use and provide new housing opportunities in the City. The emphasis of the commercial uses is

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primarily on commercial, service, medical, educational, office, and local-serving and region-serving retail. Development is intended to consist primarily of businesses on the ground floor with services and/ or housing on upper stories. Development is intended to be pedestrian-oriented with buildings close to and oriented to the sidewalk, especially at corners with through block connections that provide connections between properties and an overall modified grid of streets.

The Woodland District Master Plan requirements strive to achieve a finer-grained network of streets and paths than currently exists, while permitting property owners flexibility regarding location, alignment and design, or type of streets and paths. New streets and paths will most likely be easements, and new infill blocks will be created as a result. Once a new infill block is created, the FBC development standards will apply in the same way that they will apply to legally defined sites and street-bounded blocks within the rest of the Woodland District. FBC development standards include stormwater regulations, height limits, setbacks and frontage type requirements.

16.24.020 Uses

- A. Permitted uses. Uses permitted in the Woodland District zones are listed in Table 16.24.020-1 with a “P.” These uses are allowed if they comply with the development standards and other regulations of this Section. Listing as an allowed use does not mean that a proposed development will be granted an adjustment or other exception to the regulations of this Title.
- B. Conditional uses. Uses which are allowed if approved through the conditional use review process are listed in Table 16.24.020-1 with a “C.” These uses are allowed provided they comply with the conditional use approval criteria for that use, the development standards, and other regulations of the LMC.

Use-Specific Development Standards

Parks and Open Space

Huntamer Park, West Plaza Park, South Plaza Park, I-5 Park and Bikeway, and Civic Plaza are the current open spaces and parks in the Woodland District. These City-owned and managed facilities provide open space and natural amenities for the enjoyment of the public. The Woodland District Strategic Plan recommends reinforcing these areas through an improved network of sidewalks, bicycle lanes, paths, and multi-purpose trails. New development shall enhance these existing district open spaces. Application of the design standards for streets, paths, buildings, landscaping, and other design elements from the Hybrid Form Based Code will provide a sense of enclosure to the perimeter of the parks. Any new common open space, park, or Through Connection shall be designed to be useable for the recreation and enjoyment of the citizens.

Standards

There are no new open spaces identified for development in the Woodland District. Any new open space dedicated to the city shall be subject to the design criteria of LMC Chapter 14.23.088 Open Space and requirements of Chapter 16.48 Open Space/ Institutional District. Private open space and recreation associated with new residential or mixed-use development shall comply with the design criteria of LMC 14.088 Open Space.

Drive-Through Facilities

Where a drive-through component is proposed as part of a development, it shall meet the following regulations:

- a. The drive-through shall be accessory to the principal use of the development;
- b. The entrance to the drive-through lane shall not be on a Primary Street unless shared with the primary site access of the principal use. The drive-through lane and the drive through window shall not be visible from a Primary Street.
- c. The standards above may be met in either of the following ways:
 - i. The drive-through shall be accessed from a Secondary Street, Other Street or Through Connection, and contained within the building;
 - ii. The drive-through shall be accessed from a Secondary Street, Other Street or Through Connection, and located on the portion of the Infill Block that is farthest away from the Primary Street.

Ground Floor Residential Uses

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Where residential uses occur on the ground floor, vertical and horizontal separation is required to ensure privacy for building residents, and a high quality public realm. The minimum and maximum vertical and horizontal distance from the sidewalk is defined in the Development Standards Tables for Building and Landscape Frontage Types.

Table 16.24.020-1

Use Categories	1 – Woodland Square	2 – Pacific Avenue	3 – Master Plan
	P – Permitted	C – Conditional	
Residential Categories¹			
Household Living	P	P	P
Group Living	C	C	C
Commercial Categories			
Retail Sales and Service	P	P	P
Office	P	P	P
Institutional Categories			
Parks and Open Space	P	P	P
Educational Facilities, Government Offices, Museum; Civic Uses, Transit Uses	P	P	P
Other Categories			
Rail Lines & Utility Corridors	P	P	P

¹ Where residential uses occur on the ground floor, vertical and horizontal separation is required, see Development Standards Tables for Building and Landscape Frontage Types.

Prohibited Uses.

- A. Uses with physical and operational requirements generating substantial:
 - Truck traffic
 - Dust
 - Glare
 - Heat or vibration
 - Noise
 - Odors
- B. Uses of a character which are either not compatible with the high aesthetic standards of the area, will not enhance the marketability of the core area, or will adversely impact the city’s economic development strategies for this zone. These uses shall include, but are not limited to:

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Activities entailing movement of heavy equipment on and off the site except during construction;
Auto or truck storage as a primary use;
Cemeteries and crematoria;
Machine shops;
Motor freight terminals;
Park and ride lots;
Solid waste disposal facilities, including transfer stations, incinerators and sanitary landfills;
Stand-alone warehouse and distribution facilities.

16.24.030 General Standards Required for all Development

Conflicts

Development within the Woodland District must comply with the standards prescribed in Chapter 16.24 Woodland District Hybrid Form Based Code. These development standards are intended to implement policies in the adopted Woodland District Strategic Plan. Whenever the standards conflict with regulations elsewhere in the LMC, these standards supersede those regulations.

In the event of a conflict between any provision of Section 16.24 Woodland District Hybrid Form Based Code and any other ordinances of the City of Lacey the provisions of this chapter shall prevail with the exception of ordinances whose standards are more restrictive than those set out in this section.

Master Plan Requirements

Connectivity Master Plan

Connectivity Master Plans are required for all development within the Woodland District, except lots or parcels exempt from Connection Spacing Standards because of size, refer to Section 16.24.050 Streets, Through Connections and Connection Spacing.

Development proposals shall show conceptually how the development standards in 16.24.050, Streets, Through Connections and Connection Spacing, shall be met in relationship to adjacent property and existing streets, through connections and other paths or trails.

Connectivity Master Plans shall refer to 16.24.040 Regulating Plan, Sub-Districts And Streets, and provide review material according to 16.24.130 Development Review – Submittal Requirements, Sections A, and B.

In addition, the Connectivity Master Plan should generally indicate how open space, parking, driveways, walkways, etc., will relate or connect to adjacent parcels.

Detailed Master Plan

Detailed Master Plans are required for all development in the Master Plan Area where proposed improvements represent 25% or more of the value of the assessed market improvement value of the existing structure. For the purposes of determining code compliance, the value of proposed improvements will be based on the value of the building permit for those improvements.

Development proposals shall show conceptually how the development meets the development standards in the following section:

- 16.24.050 Streets, Through Connections and Connection Spacing
- 16.24.060 Building, Form, Siting and Site Design
- 16.24.070 Building and Landscape Frontage

Detailed Master Plans shall refer to 16.24.040-1 Regulating Plan, Subdistricts; 16.24.040-2 Regulating Plan, Designated Street Types, and 16.24.040-3 Regulating Plan, Building Heights.

Detailed Master Plans shall provide review material according to 16.24.130 Development Review – Submittal Requirements, all sections.

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Master Plan Eligibility Summary		
Eligibility	Connectivity Master Plan	Detailed Master Plan
Lot or parcel is exempt from Connectivity Standards in 16.24.050 Streets, Through Connections and Connection Spacing due to size.	Exempt	Exempt
Lot or parcel is 1) subject to the Connectivity Standards in 16.24.050 Streets, Through Connections and 2) outside the Master Plan Area, as shown on Figure 16.24.040-1 Regulating Plan Subdistricts.	Must meet Connectivity Master Plan Requirements	Exempt
Lot or parcel is 1) inside the Master Plan Area, as shown on Figure 16.24.040-1 Regulating Plan Subdistricts, and 2) proposed improvements are less than 25% of the value of the underlying development as defined above.	Must meet Connectivity Master Plan Requirements	Exempt
Lot or parcel is 1) inside the Master Plan Area, as shown on Figure 16.24.040-1 Regulating Plan Subdistricts, and 2) proposed improvements represent 25% or more of the value of the underlying development as defined above.	Must meet Detailed Master Plan Requirements	

Adjustments

There are three types of Adjustments that may be granted by staff, as described below.

1. Proportional Compliance Adjustments

Proportional Compliance Adjustments apply to lots or parcels fronting on Pacific Avenue and lots or parcels within the Woodland Square Subdistrict and Pacific Subdistrict, as illustrated in Figure 16.24.040-1 Regulating Plan, Subdistricts and Figure 16.24.040-2 Regulating Plan, Streets.

Proportional Compliance Adjustments may be granted to existing development where the value of proposed improvements fall below one of two thresholds. For the purposes of determining compliance, the value of proposed improvements shall be cumulative over the most recent five years, including calculations of all previously exempt remodels, but shall not include life/safety improvements or normal maintenance not requiring a building permit.

- a) When the value of the proposed improvements is less than 15% of the value of the assessed market improvement value of the existing structure, or when the permit value of the proposed improvement is less than \$20,000, the applicant may secure a building permit for the

proposed improvements without meeting any of the development standards in the form based code.

- b) When the value of the proposed improvements is greater than 15%, but less than 75% of the value of the existing development, the applicant must meet the development standards of the form based code for the improvement, only. Landscaping: the reconfigured portion of the site shall meet the applicable development standards. Parking lot reconfiguration and expansion: the reconfigured portion of the parking lot shall meet the applicable development standards. External facade modification: the full extent of the facade shall meet the applicable development standards.
- c) When the value of the proposed improvements is greater than 75% of the assessed market improvement value of the existing structure, or when the permit value of the proposed improvement is greater than \$5,000,000, the applicant must meet the development standards of the form based code for the new improvements and the existing building. Landscaping: the entire site shall meet the applicable development standards. Parking lot reconfiguration and expansion: the entire parking lot shall meet the applicable development standards. External facade modification: the full extent of the all facades shall meet the applicable development standards.
- d) Expansion of building footprint: the new square footage associated with the building expansion is required to meet the applicable development standards.

2. Development Standards Flexibility Adjustment

Development Standards Flexibility Adjustments may be granted to any development within the Woodland District, if staff finds that the adjusted Development Standard will perform as well as the Development Standard. Eligible Development Standards and the permitted degree of adjustment is noted in each Development Standards tables.

3. Site Plan Review Committee (SPRC) Adjustment

Any development standards which are not included in the Development Standards Flexibility Adjustment above, or which exceed the permitted degree of flexibility noted in the Development Standards tables, are eligible for review and approval through the Site Plan Review Committee (SPRC), according to Section 16.24.100 Development Review Process.

New and Existing Streets and Through Connections

Development standards of Chapter 16.24 are intended to establish a complete network of new and existing streets and Through Connections, which may take the form of local streets, multi-use paths or woonerfs. The location of new and existing streets and Through Connections and their required intersections are mapped in Figure 16.24.040-2, Regulating Plan, Designated Street Types. Connection Types permitted and maximum spacing of new and existing streets and Through Connections is established in Table 16.24.050-1, Connections and Connection Spacing. The required improvements for each street and Through Connection, including the sidewalk zones, are specified in Figures 16.24.050-1 through 16.24.050-6. When the property owner or developer is responsible for dedication of land and/ or specific constructed improvements it is noted in Figures 16.24.050-1 through 16.24.050-6.

The following table summarizes the improvements and the responsibility of each party, whether City or property owner / developer.

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Summary of Requirements	Eligible Streets
<p>Existing Built Streets</p> <p>Existing streets are required to meet requirements for sidewalk improvements, street lights, street furniture, and trees, according to Table 16.24.050-1, Table 16.24.050-2 and Figures 16.24.050-1 through 16.24.050-6, Street Types and Sidewalk Improvements. Improvements and, where noted in the Development Standards, dedication of land, is the responsibility of the property owner / development applicant</p>	<p>3rd Avenue SE 6th Avenue SE 7th Avenue SE Pacific Avenue SE Sleater Kinney Road SE College Street SE</p>
<p>Future Streets and Through Connections</p> <p>Right of Way is dedicated by property owner / development applicant. The City constructs the street to City standards as established by Table 16.24.050-1, Table 16.24.050-2 and Figures 16.24.050-1 through 16.24.050-6, Street Types. An interim bike-pedestrian trail may be required on the dedicated Right of Way, prior to completion of the street improvements.</p>	<p>Unbuilt 4th Avenue SE Unbuilt segment of Golf Club Road SE Unbuilt segment of 10th Avenue SE All Through Connections and Other Streets outside of the Master Plan Subdistrict</p>
<p>Master Planned Streets</p> <p>Master Planned Streets shall be located and constructed according to an approved Master Plan that meets the requirements of a Connectivity Master Plan or a Detailed Master Plan, as applicable. The property owner or developer is responsible for all required street or Through Connection improvements.</p>	<p>All Other Streets and Through Connections within the Master Plan Subdistrict</p>

Environmental Performance

- A. It shall be the responsibility of the operator and/or the proprietor of any proposed use to provide such evidence and technical data as the enforcing officer and/or site plan review committee may require to demonstrate that the use or activity is or will be in compliance with the environmental performance standards of Chapter [16.57](#) LMC.
- B. Failure of the enforcing officer and/or site plan review committee to require such information shall not be construed as relieving the operator and/or the proprietor from compliance with Chapter 16.57 LMC, environmental performance standards.
- C. All stormwater runoff shall be retained and disposed of on site or disposed of in a system designed for such runoff and which does not flood or damage adjacent properties. Systems designed for runoff retention and control shall comply with specifications provided by the city and shall be subject to its review and approval, and shall, moreover, comply with Chapter [15.22](#) LMC pertaining to community facilities.

TABLE 16.24.030-1, DISTRICT WIDE DEVELOPMENT STANDARDS

Section / Description	1 – Woodland Square	2 –Pacific Avenue	3 – Master Plan Areas
Stormwater	All projects shall meet the amended 2010 City of Lacey Stormwater Design Manual which have square footage thresholds for development and redevelopment (2,000 square feet and 5,000 square feet of new or redeveloped impervious surfaces). No requirement for areas under 2,000 square feet; between 2,000 and 5,000 square feet required to keep stormwater on site, and over 5,000 square feet full treatment and infiltration is required.		
Tree Preservation	<p>Tree protection professional report required. The city’s tree protection professional shall review the site and provide a report analyzing the site for tree protection consistent with the requirements of this chapter. The report should provide information important to urban forest management and options for consideration when developing preliminary designs. The report should suggest options for design to best achieve the purposes of the Urban Forest Management Plan and this chapter. The report shall include but shall not be limited to:</p> <ol style="list-style-type: none"> a. An analysis of technical information requested by the review body related to trees and forest practices; b. Analysis of what portion of the site is best for designation of the tree tract if required, considering the intent of this chapter, soil type, topography, tree species, health of trees and reasonable project design limitations; c. Recommendations for saving of individual tree specimens based upon the intent of this chapter, soil type, topography, tree species, health of trees, and reasonable project design limitations; d. A plan for protection of trees to be saved during construction including placement of construction fences, monitoring of construction activity and other measures necessary to ensure adequate tree protection; e. Consideration of the location of roads, other infrastructure, and buildings and potential options for alternative locations, if applicable, to best satisfy the purposes of the Urban Forest Management Plan; f. A timeline for tree protection activity; g. The final tree protection plan should be prepared on the site grading plan. All tree protection fences, trees to be saved, and trees to be removed should also be shown on the site demolition plan. Necessary save tree pruning and selective thinning within tree tracts shall be detailed and trees marked as such. The tree protection plan and demolition plan should be part of 		<p>Development standards may be adjusted on a case by case basis to protect priority trees:</p> <p>Priority tree types: Trees to be protected must be healthy, windfirm, and appropriate to the site at their mature size, as identified by a qualified professional forester. In designing a development project, the applicant shall protect the following types of trees in designated tract(s) in the following order of priority:</p> <ol style="list-style-type: none"> 1. Historical trees. Trees designated as historical trees under <u>LMC 14.32.072</u>. 2. Specimen trees. Unusual, rare, or high quality trees. 3. Critical area buffer. Trees located adjacent to critical area buffers. 4. Significant wildlife habitat. Trees located within or buffering significant wildlife habitat. 5. Other high quality individual trees or groves of trees.

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Section / Description	1 – Woodland Square	2 –Pacific Avenue	3 – Master Plan Areas
	<p>the submittal to the city of Lacey and shall be approved by the tree protection professional. The tree protection plan shall be part of the contractor bid package and a copy of the tree protection plan shall be available to the contractors on site at all times during logging, clearing, and construction.</p>		

TABLE 16.24.030-2, Low Impact Development Approaches (LIDA)

General

Low Impact Development Approach (LIDA). A stormwater management and land-development strategy applied at the scale of the block and the scale of the parcel that emphasizes conversation and use of onsite natural features integrated with engineered, small-scale hydrologic controls to more closely mimic predevelopment hydrologic functions.

Urban Mixed-use

Low-impact Development Approaches (LIDA) should reflect the character of place. In the most urban areas of the Woodland District, the function of rainwater detention and stormwater re-infiltration should be intense and highly structured. LIDA techniques consistent with this context include green roofs, rainwater harvesting, infiltration planters, permeable paving, and detention vaults.

Urban Mixed Use LID Approach	Example	Where Permitted
<p>Urban Mixed Use Type A</p> <p>For urban sidewalks where there is on-street parking and street furniture. Designed to accommodate frequent pedestrian traffic between parked cars and retail or service commercial. Planted area is confined to a portion of the furnishings zone of the sidewalk, and is limited in length.</p>		<p>6th Avenue Golf Club Road Pacific Avenue Other Streets Through Connections Infill Block site area</p>

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<p>Urban Mixed Use Type B</p> <p>For urban sidewalks which include curb extensions, such as at corner curb ramps.</p>		<p>Golf Club Road Pacific Avenue Other Streets Through Connections Infill Block site area</p>
<p>Urban Mixed Use Type C</p> <p>For mixed use and residential courtyards and forecourts.</p>		<p>Infill Block site area</p>

Urban Residential

In the more residential settings of the Woodland District the function of rainwater detention and stormwater re-infiltration may include LIDA techniques such green roofs, rainwater harvesting, infiltration planters, flow-through planters, rainwater gardens, and permeable paving.

Urban Residential LID Approach	Example	Where Permitted
<p>Urban Residential Type A</p> <p>Informal rainwater gardens and planted area suitable for courtyard or forecourt edges or Porch-Stoop-Terrace Frontages. May also be used within the horizontal separation zone required for ground floor residential.</p>		<p>Golf Club Road Pacific Avenue Other Streets Through Connections Infill Block site area</p>
<p>Urban Residential Type B</p> <p>For urban sidewalks in residential areas where there is on-street parking and street furniture. Designed to accommodate pedestrian traffic between parked cars and residential entries. Planted area may be used in conjunction with required street trees and informally planted with native plants.</p>		<p>6th Avenue Golf Club Road Pacific Avenue Other Streets Through Connections Infill Block site area</p>

Through Connections and Parking Lots

Parking lots, private streets, and multi-use paths should be designed to detain and redirect stormwater runoff. LIDA design includes bioretention in vegetated swales, flow-through planters, and rainwater gardens. Pervious pavement is an effective alternative to conventional curbs, catch basins, sewer pipes, and treatment facilities.

Parking Lot LID Approach	Example	Where Permitted
<p>Parking Lot LID Approach A</p> <p>Contained swale or rainwater garden suitable for internal parking lot landscaping, and to fulfill parking lot perimeter landscaping requirement wherever a parking lot abuts a street or Through Connection.</p>		<p>Pacific Avenue Other Streets Through Connections Infill Block site area</p>
<p>Parking Lot LID Approach B</p> <p>Suitable for internal parking lot landscaping. May be used in conjunction with required parking lot tree planting.</p>		<p>Infill Block site area</p>
<p>Parking Lot LID Approach C</p> <p>Swale with native plants including small trees and shrubs with vertical habit. Suitable for internal parking lot landscaping, and to fulfill parking lot perimeter landscaping requirement wherever a parking lot abuts a street or Through Connection.</p>		<p>Pacific Avenue Other Streets Through Connections Infill Block site area</p>

16.24.040 Regulating Plan

Description of the Regulating Plan

The Regulating Plan for the Woodland District is set out in Figure 16.24.040-1, Regulating Plan, Sub-districts; Figure 16.24.040-2, Regulating Plan, Street Types; and Figure 16.24.040-3, Regulating Plan, Building Heights. The Regulating Plans specify the Sub-districts, Street Types and Building Heights that apply to each lot, parcel of land, or Infill Block.

The Street Type that applies to a specific street or section of a street is indicated by the color and corresponding name shown on the Table 16.24.050-2, Overview of Street and Through Connection Types.

On Figure 16.24.040-2, Regulating Plan, Street Types, where the Street Type, as indicated by color, extends through an intersection, that Street Type shall be considered as the higher order Street and shall apply to the intersection, and to the intersecting Street until a distance of 100 feet from the confluence of lot lines at the corner, or until the next lot line away from the corner, whichever distance is shorter.

Allowable building heights are shown on Figure 16.24.040-3, Regulating Plan, Building Heights.

FIGURE 16.24.040-1, REGULATING PLAN, SUB-DISTRICTS



Figure 16.24.040-1, Regulating Plan. Subdistricts

FIGURE 16.24.040-2, REGULATING PLAN, STREET TYPES



Figure 16.24.040-2, Regulating Plan, Designated Street Types

FIGURE 16.24.040-3, REGULATING PLAN, BUILDING HEIGHTS

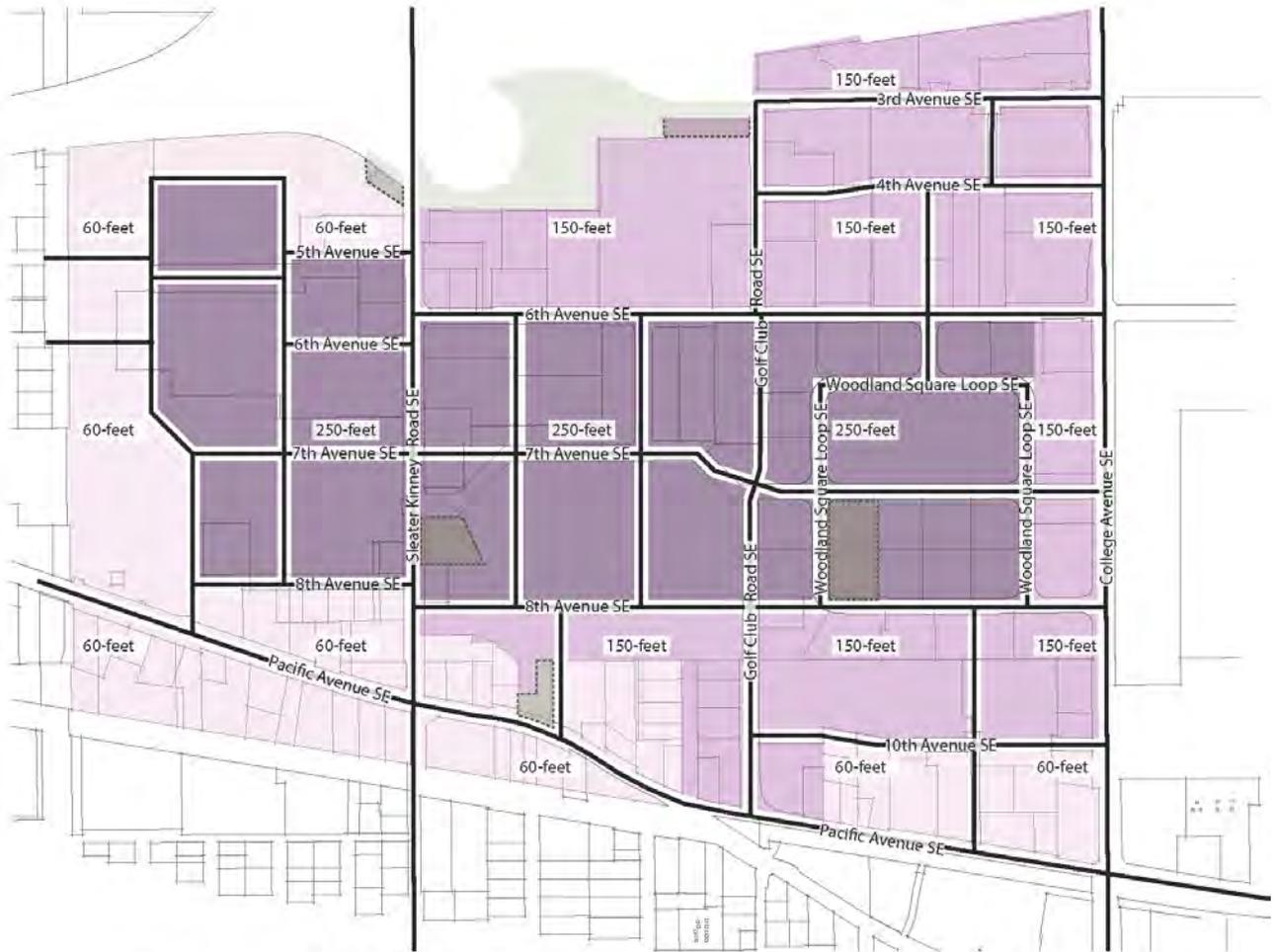


Figure 16.24.040-3, Regulating Plan, Building Heights

16.24.050 Streets, Through Connections and Connection Spacing

TABLE 16.24.050-1, CONNECTIONS AND CONNECTION SPACING

	Woodland Square	Pacific Avenue	Master Plan Areas
BLOCKS AND CONNECTIONS			
Maximum Block Length	Auto: 450 feet (1) Pedestrian: 330 feet	Auto: 450 feet (3) Pedestrian: 330 feet South of Pacific Avenue, Auto: 200 feet Pedestrian: 200 feet	Auto: 450 feet Pedestrian: 330 feet
Maximum Block Perimeter	Auto: 1,800 feet (1) Pedestrian: 1,320 feet	Auto: 1,800 feet (2) Pedestrian: 1,320 feet South of Pacific Avenue, NA	Auto: 1,800 feet (1) Pedestrian: 1,320 feet
Additional Through-Block Connections	Required for block faces longer than 450 feet	Required for block faces longer than 450 feet	Required for block faces longer than 450 feet
Vehicular Entrances	Driveways permitted except where noted Min. 40 feet separation from intersection Max. avg. 1 driveway per 100 feet of block frontage Maximum width: 24 feet	Driveways permitted except where noted Min. 40 feet separation from intersection Max. avg. 1 driveway per 100 feet of block frontage Maximum width: 24 feet (3)	Driveways permitted except where noted Min. 40 feet separation from intersection Max. avg. 1 driveway per 100 feet of block frontage Maximum width: 24 feet
Connection Hierarchy and Primary Frontage	If one of the designated streets or Through Connections bounding an Infill Block is a Primary Street, the Primary Street Frontage of the Infill Block or lot shall be the Primary Street. If none of the designated streets or Through Connections is a Primary Street, the primary frontage shall be the Secondary Street. (4)		
Through Block Connection Types Permitted	F – Through Block Connection	F – Through Block Connection	F – Through Block Connection

(1) Adjustable by 5 percent

(2) Adjustable by 10 percent

(3) Adjustable by 20 percent

(4) Proportional Compliance Adjustment: for properties south of Pacific Avenue the Through Connection of the driveway access and drive aisle may be exempt from Frontage Requirements.

TABLE 16.24.050-2, OVERVIEW OF STREETS AND THROUGH CONNECTION TYPES

Section / Description	6th Ave	Golf Club	Pacific	College, Sleater Kinney	All Other Streets	Through Block Connection
Frontage	Primary	Primary	Primary	Secondary	Secondary	Secondary
Type	Collector / Commercial	Collector / Commercial	Arterial	Arterial	Local Streets	Local street or path
Aesthetic Character / Identity	Mixed-Use Main Street	Urban Residential Main Street	Varies	Woodland District Gateways	Varies	Varies
Building and Landscape Frontage Types Permitted	Linear Forecourt Low Wall and Trellis Urban Wall or Fence	Linear Forecourt Porch-Stoop-Terrace Low Wall and Trellis Urban Wall or Fence	Linear Forecourt Porch-Stoop-Terrace Landscape Building Low Wall and Trellis Urban Wall or Fence Landscape Setback	Linear Forecourt Porch-Stoop-Terrace Landscape Building Low Wall and Trellis Urban Wall or Fence Landscape Setback	Linear Forecourt Porch-Stoop-Terrace Landscape Building Low Wall and Trellis Urban Wall or Fence Landscape Setback	Linear Forecourt Porch-Stoop-Terrace Landscape Building Low Wall and Trellis Urban Wall or Fence Landscape Setback
Role in the Network	District Connector	District Connector	Regional Connector	Regional Connector	Bike, pedestrian, local vehicular connectivity	Bike, pedestrian, local vehicular connectivity
Design Speed	25 mph	Under 20 mph	25 mph	25 mph	Under 20 mph	Under 20 mph
Right-of-Way or Easement Width	81 to 87 feet	60 to 64 feet	90 to 96 feet	Varies	Varies	Varies
Location of Build To Line	At front property line	At front property line	5 feet back from property line	At front property line	At front property line	NA
Curb-to-Curb Width	42 feet	36 feet	62 to 68 feet	Varies	Varies	Varies
Travel	2	2	4	4	2	Optional

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Section / Description	6th Ave	Golf Club	Pacific	College, Sleater Kinney	All Other Streets	Through Block Connection
Lanes (number)						
Travel Lane Width	11 feet	10 feet	12 feet	Varies	9.5 feet for Local Streets 6 to 8 feet for Queuing Streets	10 feet (maximum)
Center Turn Lane Width	14 feet	NA	14 feet	Varies	Varies	NA
Parking Lane Width	8.5 feet	8 feet	NA	NA	NA	8-30 feet (optional; head-in, diagonal, parallel, or combination permitted)
Bike Facilities	None	Shared	None	NA	NA	Shared street or shared-use path
Sidewalk Width	14 to 20 feet	12 to 14 feet	14 feet	Varies	Varies	5 feet (minimum) each side, or 10 feet (minimum) one side, or 10 feet minimum (no travel lane)
Planting Street Width	Varies	Varies	Varies	Varies	Varies	6 feet (minimum)
Planted Median Width	14 feet	NA	14 to 20 feet	NA	NA	NA

FIGURES 16.24.050-1 THROUGH 6, STREET AND THROUGH CONNECTION TYPES

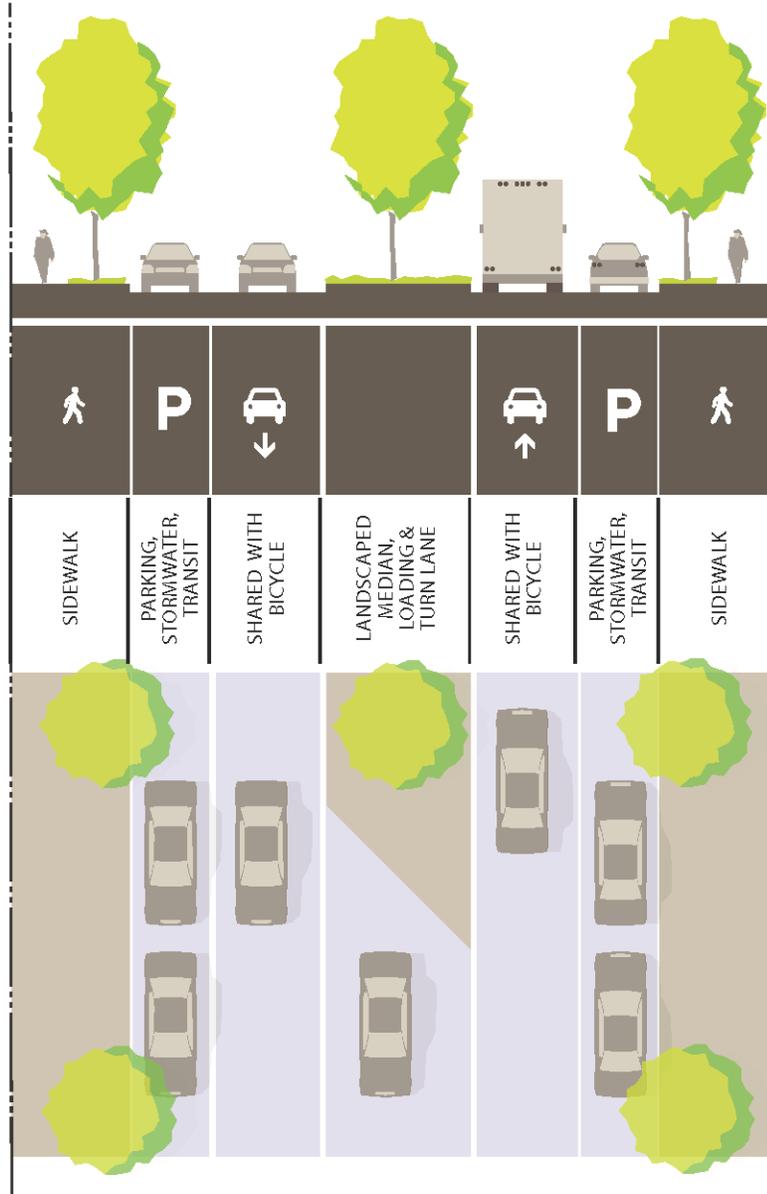


Figure 16.24.050-1, 6th Avenue

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6th Ave	
Right-of-Way or Easement Width	81 to 87 feet
Location of Build To Line	At front property line
Curb-to-Curb Width	42 feet
Travel Lanes (number)	2
Travel Lane Width	11 feet
Center Turn Lane Width	14 feet
Parking Lane Width	8.5 feet
Bike Facilities	None
Sidewalk Width	14 to 20 feet
Planting Street Width	Varies
Planted Median Width	14 feet

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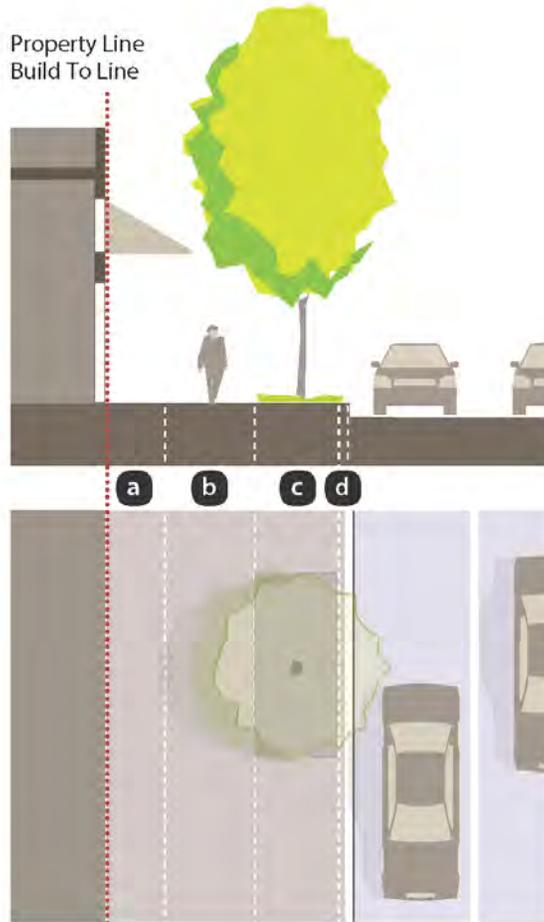


Figure 16.24.050-2, 6th Avenue Sidewalk Improvements

6th Avenue Sidewalk Improvements

Applicant is responsible for constructing improvements to the sidewalk when development is approved.

	Sidewalk Zones	Minimum Dimensions	Required Improvements
a	Frontage Zone	1.5 feet	Concrete sidewalk
b	Pedestrian Through Zone	6.0 feet	Concrete sidewalk
c	Street Furniture Zone	4.0 feet	Concrete sidewalk, street trees, tree grates
d	Curb Zone	6 inches	Cast-in-place concrete curb and gutter

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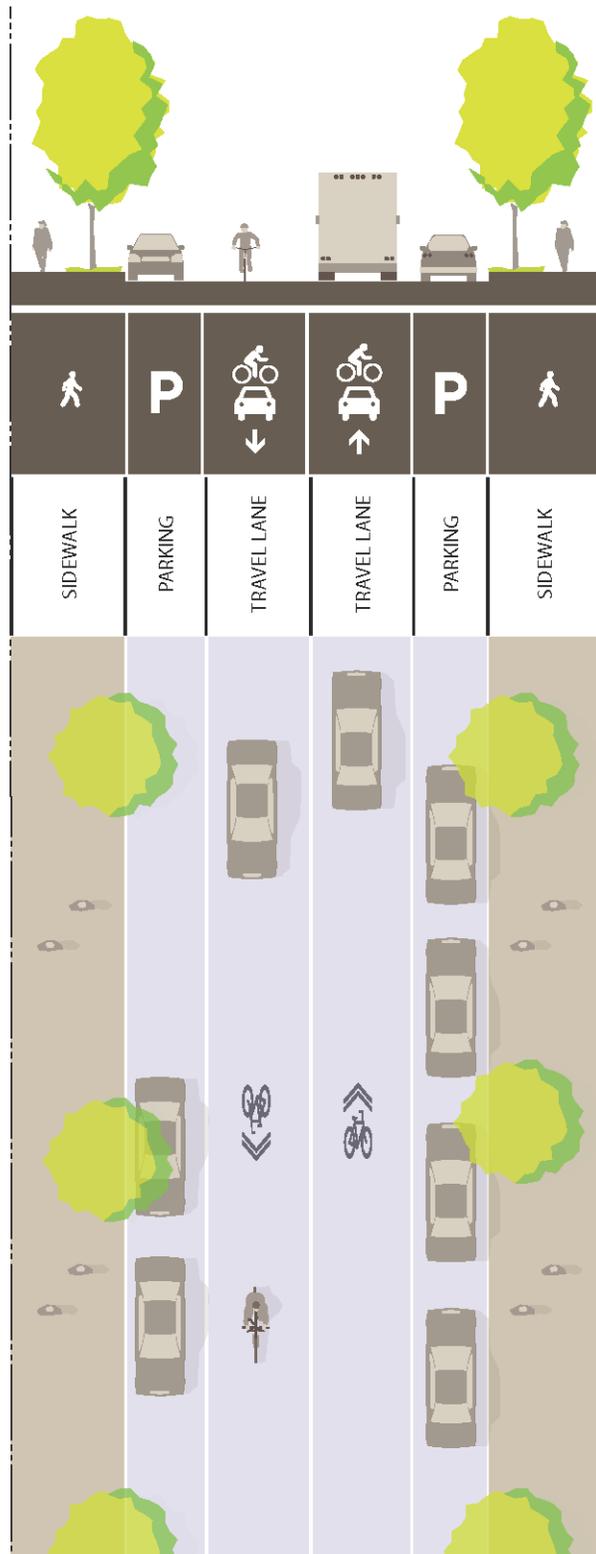


Figure 16.24.050-3, Golf Club Road

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Golf Club Road	
Right-of-Way or Easement Width	60 to 64 feet ¹
Location of Build To Line	At front property line
Curb-to-Curb Width	36 feet
Travel Lanes (number)	2
Travel Lane Width	10 feet
Center Turn Lane Width	NA
Parking Lane Width	8 feet
Bike Facilities	Shared
Sidewalk Width	12 to 14 feet
Planting Street Width	Varies
Planted Median Width	NA

¹ **Golf Club Road Extension.** Between 6th Avenue and 7th Avenue, applicant shall, upon development approval, dedicate land for the Right of Way and construct interim improvements according to Through Block Connections Development Standards. City is responsible for ultimate improvements according to the Golf Club Development Standards.

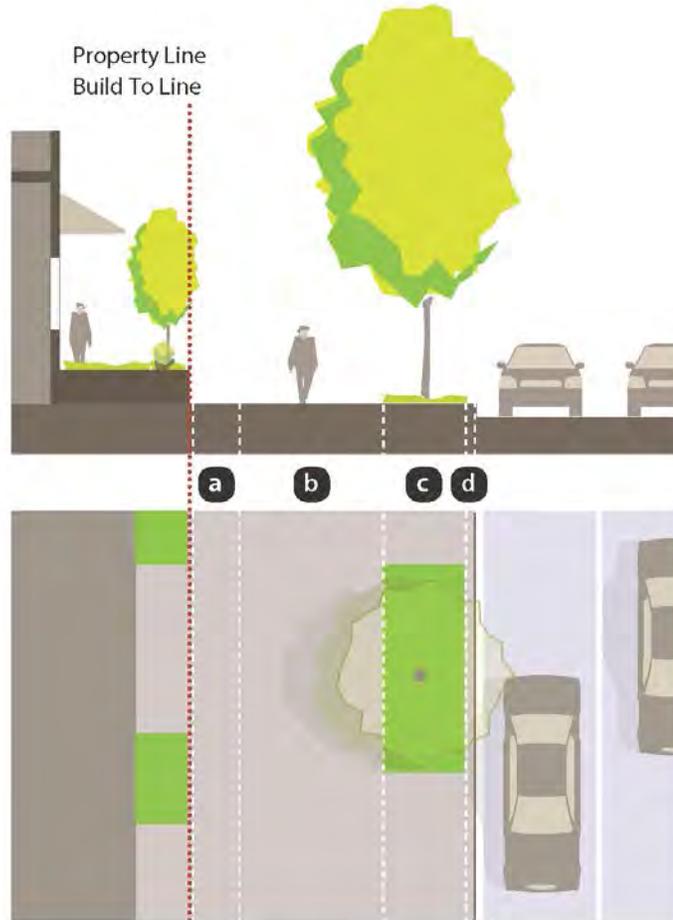


Figure 16.050-4, Golf Club Road Sidewalk Improvements

Golf Club Road Sidewalk Improvements

Applicant is responsible for constructing improvements to the sidewalk when development is approved.

	Sidewalk Zones	Minimum Dimensions	Required Improvements
a	Frontage Zone	1.5 feet	Concrete sidewalk
b	Pedestrian Through Zone	6.0 feet	Concrete sidewalk
c	Street Furniture Zone	4.0 feet	Street trees
d	Curb Zone	6 inches	Cast-in-place concrete curb and gutter

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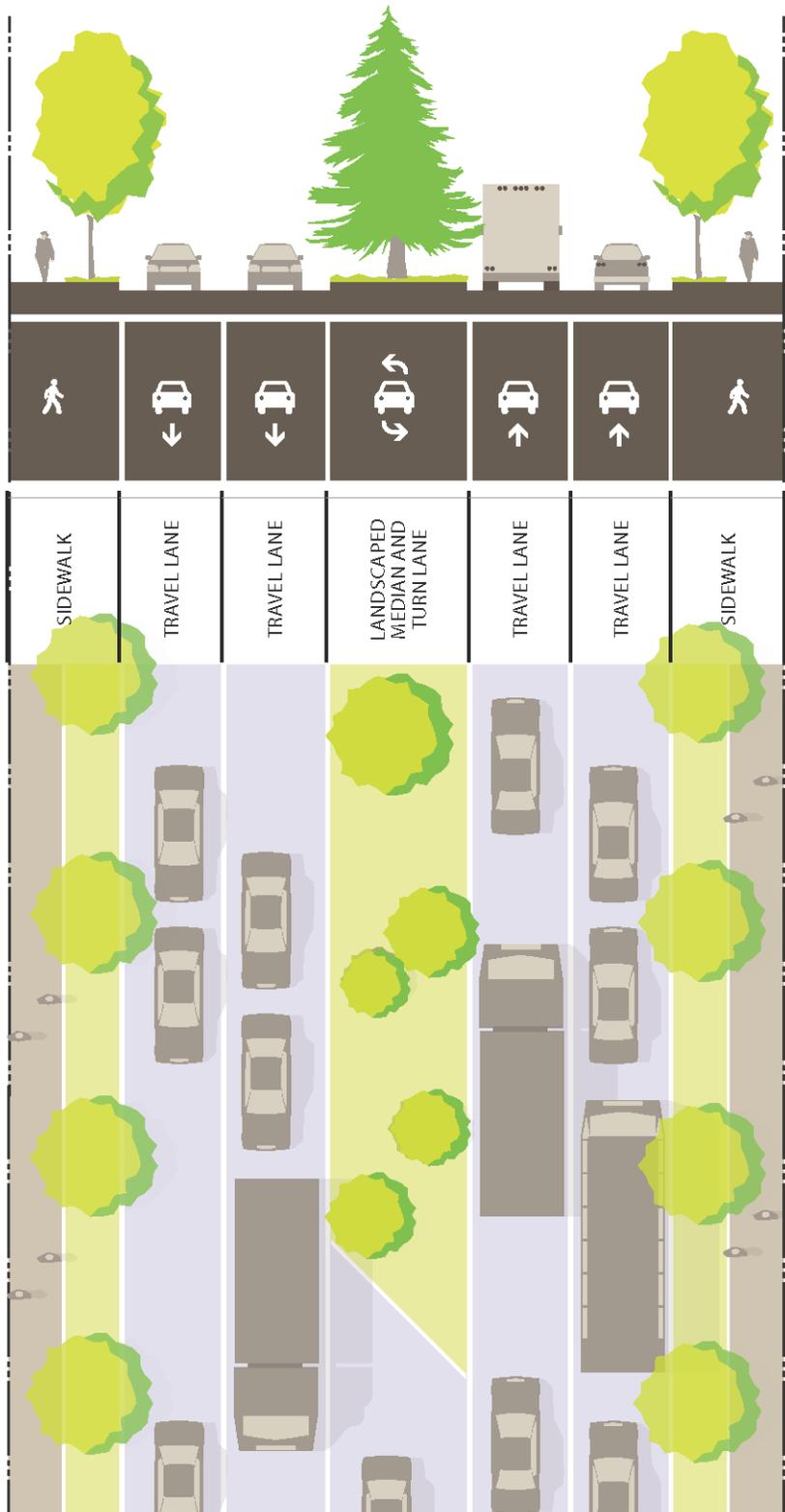


Figure 16.24.050-5, Pacific Avenue

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Pacific Avenue	
Right-of-Way or Easement Width	90 to 96 feet
Location of Build To Line	5 feet back from property line
Curb-to-Curb Width	62 to 68 feet
Travel Lanes (number)	4
Travel Lane Width	12 feet
Center Turn Lane Width	14 feet
Parking Lane Width	NA
Bike Facilities	None
Sidewalk Width	14 feet
Planting Street Width	Varies
Planted Median Width	14 to 20 feet ¹

¹ Where Right of Way constraints do not permit a center turn lane, the Planted Median Width requirements do not apply

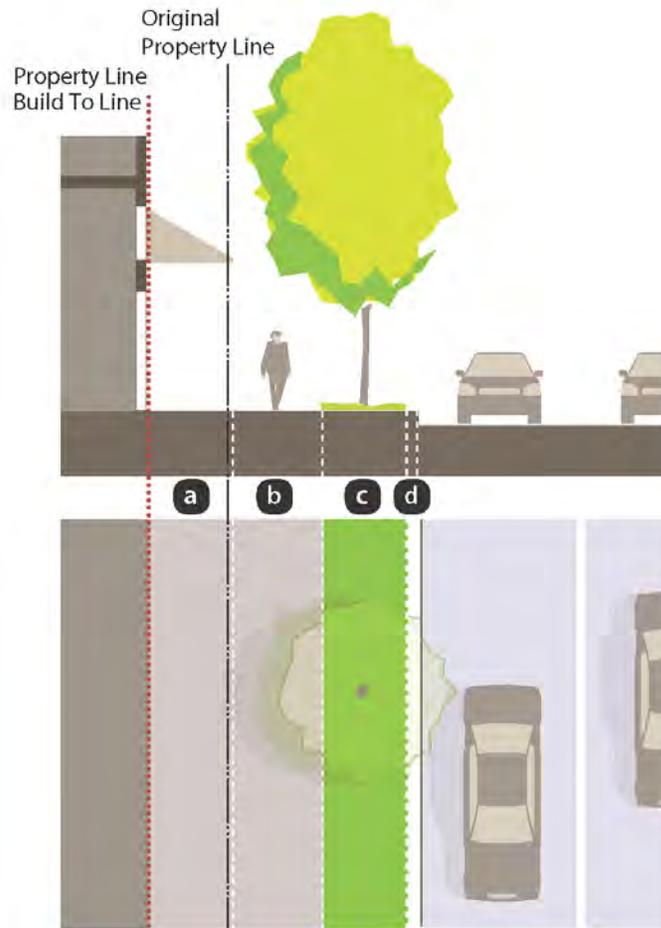


Figure 16.24.050-6, Pacific Avenue Sidewalk Improvements

Pacific Avenue Sidewalk Improvements

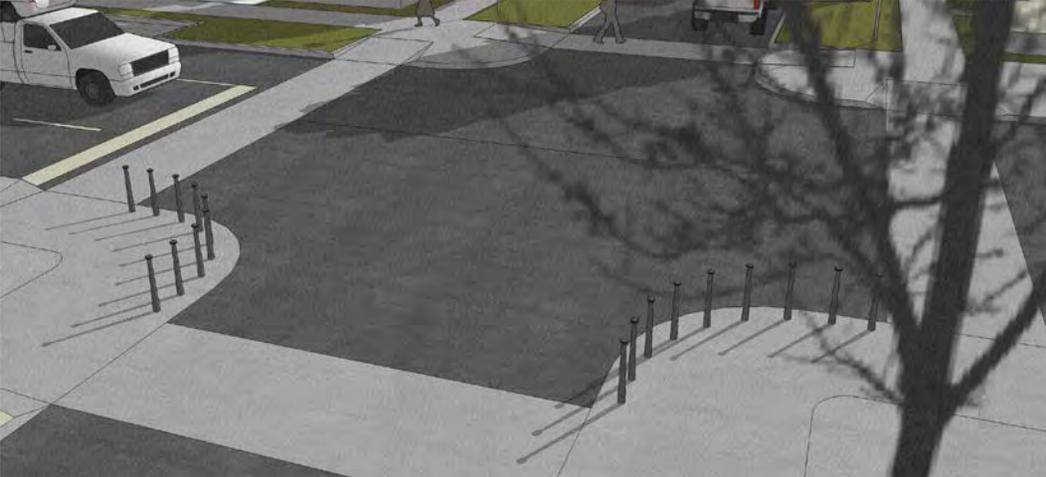
Applicant is responsible for dedicating 5 feet to the Right of Way and for constructing improvements to the sidewalk when development is approved.

	Sidewalk Zones	Minimum Dimensions	Required Improvements
a	Frontage Zone	5.0 feet	Concrete sidewalk
b	Pedestrian Through Zone	5.0 feet	Concrete sidewalk
c	Street Furniture Zone	4.0 feet	Street trees, ground cover
d	Curb Zone	6 inches	Cast-in-place concrete curb and gutter

TABLE 16.24.050-3, Street and Through Connection Crossing Approaches

Crossing Description	Example	Location
<p>Urban Plaza Intersection</p> <p>Raise the intersection to the level of the adjacent sidewalks. Tighten intersection curb radii for slower turning speeds for motorized vehicles. Install vertical projections such as bollards and planters to channel the automobiles and increase the security and safety of pedestrians and bicyclists.</p> <p>Use textured paving and/ or contrasting colors to promote a distinctive sense of place. Install unit pavers, textured paving, or other distinctive materials or contrasting colors to the flat surface of the intersection plaza.</p> <p>Vertical speed control elements shall be marked with a warning sign advising drivers.</p>	 <p>The image shows a 3D architectural rendering of a raised urban plaza intersection. The intersection is circular and features a textured paving pattern, likely unit pavers, which contrasts with the surrounding flat road surface. Several bollards are installed around the perimeter of the plaza to channel traffic and provide safety for pedestrians and bicyclists. A white SUV is shown driving through the plaza, and a white pickup truck is visible on the adjacent street. The surrounding area includes sidewalks, trees, and a street lamp, illustrating the integration of the plaza into the urban environment.</p>	<p>Required</p> <p>6th Avenue intersection with Golf Club Road¹</p> <p>Optional</p> <p>Pacific Avenue</p> <p>Other Streets</p> <p>Through Connections</p> <p>¹ City is responsible for construction of raised table intersection once Golf Club Road extension is completed.</p>

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Crossing Description	Example	Location
<p>Urban Corner</p> <p>Tighten intersection curb radii and encourage slower turning speeds for motorized vehicles. Eliminate the change in grade between sidewalk and drive lane to prioritize pedestrian movement and mark a unique spot in the Woodland District. Flare the curb to meet the grade of the street along the full extent of the radius of the corner. Use bollards to protect pedestrians from automobile turning movements.</p>	 An aerial photograph of a street corner. The curb is flared to match the street's grade. A series of bollards are installed along the sidewalk to protect pedestrians from turning vehicles. A white pickup truck is visible on the street. The sidewalk is marked with a unique spot.	<p>Required</p> <p>6th Avenue</p> <p>Golf Club Road between 6th and 7th Avenues</p> <p>Optional</p> <p>Pacific Avenue</p> <p>Other Streets</p> <p>Through Connections</p>

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Crossing Description	Example	Location
<p>Pedestrian Corner</p> <p>Install curb extensions to visually narrow the street and alert drivers to exercise more care. Place curb extensions to create shorter and safer crossings for pedestrians. Take advantage of curb extensions to increase the available public realm space for street furniture, benches, street trees, and other amenities.</p> <p>Curb extensions should be installed wherever on-street parking is provided. Combine stormwater management features into curb extensions at corners.</p> <p>Install curb extensions at street crossings to support pedestrian safety. Integrate flow-through planters and rain gardens, on-street parking, parklets, and bicycle parking corrals into the curb extension.</p>		<p>Required</p> <ul style="list-style-type: none"> Golf Club Road south of 7th Avenue Golf Club Road north of 6th Avenue Pacific Avenue Other Streets Through Connections in Woodland Square Subdistrict Through Connections in Master Plan Areas <p>Optional</p> <ul style="list-style-type: none"> All Other Streets Through Connections

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Crossing Description	Example	Location
<p>Urban Mid-block Crossing</p> <p>Create shorter and safer crossings for pedestrians. Raise the level of the walkway to match the level of the adjacent sidewalk to prioritize pedestrian crossing and alert drivers to the mid-block crossing. Install unit pavers or other distinctive materials to the flat surface to further define the speed table.</p> <p>Vertical speed control elements shall be marked with a warning sign advising drivers.</p>		<p>Required</p> <ul style="list-style-type: none"> Golf Club Road south of 7th Avenue Golf Club Road north of 6th Avenue Pacific Avenue Other Streets within the Woodland Square Subdistrict Other Streets within the Master Plan Areas Through Connections in Woodland Square Subdistrict Through Connections in Master Plan Areas

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Crossing Description	Example	Location
<p>Mid-block Crossing with Pedestrian Refuge</p> <p>Where streets have more than two travel lanes, pedestrian crossings shall include a pedestrian refuge within the median to provide an additional measure of safety. Incorporate planted beds, flow-through planters and rain gardens within the median island.</p>	 An architectural rendering of a city street intersection. The scene shows a multi-lane road with a central median island. A pedestrian crossing with white stripes is visible. A person is walking across the street, and another person is standing on the sidewalk. The median island features a planted bed with trees and modern streetlights. The background shows a modern building and a clear sky.	<p>Required</p> <p>6th Avenue Pacific Avenue</p> <p>Optional</p> <p>Other Streets Through Connections</p>

TABLE 16.24.050-4, Traffic Calming Approaches

Traffic Calming Description	Example	Where Permitted
<p>Urban Woonerf</p> <p>Urban Shared Space Streets function foremost as public space for shopping, commerce, culture, socializing, and recreation. The design speed of an Urban Shared Space Street is 18 mph.</p> <p>Install flush textured or pervious pavement to reinforce the priority of the pedestrian. Special pavements, especially unit pavers shall be selected for regional climate, durability, and maintenance. Sidewalks and street are installed at the same elevation (level). Align drainage channels with center or along the flush curb of the Urban Shared Space Streets.</p> <p>Provide continuous sidewalks on both sides of Urban Shared Space Streets with three distinct zones: Frontage Zone, Pedestrian Through Zone, and Street Furniture Zone. Install street furniture, including bollards, benches, planters, and bicycle parking to functionally separate cars from the pedestrian realm of the sidewalk. Provide on-street curbside parking.¹</p>		<p>Optional</p> <p>Golf Club Road</p> <p>Other Streets</p> <p>Optional, without continuous sidewalk requirement</p> <p>Through Connections¹</p> <p>¹Through Connections are exempt from the requirement for continuous sidewalk and on street parking.</p>

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Traffic Calming Description	Example	Where Permitted
<p>Curbed Street – Bulb-outs</p> <p>Curb extensions are used at intersections to shorten the distance between curbs at pedestrian crossings. Use curb extensions to integrate parking lane materials and treatments, such as permeable paving. Install curb extensions wherever on-street parking is integrated to increase visibility, reduce the crossing distance, provide extra queuing space, and allow for enhancements, such as seating or greenery. Combine stormwater management features such as bio-swales or rain gardens with curb extensions to reduce the impervious surface area of the street.</p> <p>Provide continuous sidewalks on both sides of Curbed Streets with four distinct zones: Frontage Zone, Pedestrian Through Zone, Street Furniture Zone, and Curb Zone. Install street furniture, including bollards, benches, planters, and bicycle parking to functionally separate cars from the pedestrian realm of the sidewalk.</p> <p>Parklets are ideal for curbed streets with active storefronts, foot traffic, and retail activity.</p> <p>Provide on-street curbside parking.</p>		<p>Required</p> <p>6th Avenue intersection with Golf Club Road</p> <p>Pacific Avenue</p> <p>Other Streets</p> <p>Through Connections¹</p> <p>¹Through Connections are exempt from the requirement for continuous sidewalk and on street parking.</p>

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Traffic Calming Description	Example	Where Permitted
<p>Queuing Street (Yield Street)</p> <p>Local streets in residential neighborhoods are also spaces for play and leisure. Provide safe and inviting place to walk with direct access to destinations.</p> <p>Two-way yield streets are suited to residential areas where drivers are expected to travel at low speeds.</p> <p>Provide continuous sidewalks on both sides of Queuing Streets with four distinct zones: Frontage Zone, Pedestrian Through Zone, Street Furniture Zone, and Curb Zone. Install street furniture, including bollards, benches, planters, and bicycle parking to functionally separate cars from the pedestrian realm of the sidewalk.</p> <p>Driveways shall be constructed to eliminate intrusion upon the sidewalk. Sidewalk materials and grade shall be maintained across driveways.</p> <p>Use the planted furniture zone of the sidewalk for street trees, bio-swales, and rain gardens.</p> <p>Install curb extensions at intersections to maintain safe travel speeds and reinforce the residential nature of the street.</p> <p>Install curb extensions at mid-block to slow traffic speeds and add public space. Install vertical speed control devices like raised crosswalks and mid-block crossings to encourage safe speeds and meter through traffic.</p> <p>Provide on-street curbside parking.</p>		<p>Optional</p> <p>Golf Club Road</p> <p>Other Streets</p> <p>Through Connections¹</p> <p>¹Through Connections are exempt from the requirement for continuous sidewalk and on street parking.</p>

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Traffic Calming Description	Example	Where Permitted
<p>Chicane Street</p> <p>Chicane streets are suited to residential areas where drivers are expected to travel at low speeds.</p> <p>Driveways shall be constructed to eliminate intrusion upon the sidewalk. Sidewalk materials and grade shall be maintained across driveways.</p> <p>Use the planted furniture zone of the sidewalk for street trees, bio-swales, and rain gardens.</p> <p>Install curb extensions at intersections to maintain safe travel speeds and reinforce the residential nature of the street.</p> <p>Provide continuous sidewalks on both sides of Chicane Streets with four distinct zones: Frontage Zone, Pedestrian Through Zone, Street Furniture Zone, and Curb Zone. Install street furniture, including bollards, benches, planters, and bicycle parking to functionally separate cars from the pedestrian realm of the sidewalk.</p> <p>Install curb extensions at mid-block to slow traffic speeds and add public space. Install vertical speed control devices like raised crosswalks and mid-block crossings to encourage safe speeds and meter through traffic.</p>		<p>Optional</p> <p>Golf Club Road</p> <p>Other Streets</p> <p>Through Connections¹</p> <p>¹Through Connections are exempt from the requirement for continuous sidewalk and on street parking.</p>

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Traffic Calming Description	Example	Where Permitted
<p>Woonerf – Residential</p> <p>Woonerf – Residential streets are low-volume residential streets function foremost as public space for recreation, socializing, and leisure. The design speed of an Woonerf – Residential street is 12 mph. Identify Woonerf – Residential streets with signage indicating that motorists must yield and the pedestrian has priority of movement. Mark entrances to Woonerf – Residential streets with tactile warning strips that alert both drivers and pedestrians. Install flush textured or pervious pavement reinforce the priority of the pedestrian. Special pavements, especially unit pavers shall be selected for regional climate, durability, and maintenance. Sidewalks and street are installed at the same elevation (level). Align drainage channels with center or along the flush curb of the Woonerf – Residential street.</p> <p>Provide continuous sidewalks on both sides of Woonerf – Residential street with four distinctive zones: Frontage Zone, Pedestrian Through Zone, Street Furniture Zone, and Curb Zone. Install street furniture, including bollards, benches, planters, and bicycle parking to functionally separate cars from the pedestrian realm of the sidewalk.</p> <p>Stagger blocks of on-street parking and landscaping to create a chicane effect. Install street furniture, including bollards, benches, planters, and bicycle parking to functionally separate cars from private space.</p>		<p>Optional</p> <p>Other Streets</p> <p>Through Connections¹</p> <p>¹Through Connections are exempt from the requirement for continuous sidewalk and on street parking.</p>

16.24.060 Building, Form, Siting and Site Design

TABLE 16.24.060-1, BUILDING FORM, SITING AND MASSING STANDARDS

	Woodland Square	Pacific Avenue	Master Plan Areas
Applicability	Areas bounded by designated Street Types (6 th Avenue, Golf Club Road, Pacific Avenue), Secondary Streets or Through Connections shall be designated as an Infill Block, lot or parcel and subject to the Development Standards for Building Form, Siting and Massing		
Frontage Hierarchy	<p>The Primary Street Facing Frontage is the edge of the lot, parcel or Infill Block adjacent to the Primary Street, except where no Primary Street exists, then the Primary Street Facing Frontage is defined as the edge adjacent to the Secondary Street.</p> <p>Other edges of a lot, parcel or Infill Block that abut an Other Street or a Through Connection must meet Development Standards for Secondary Streets.</p> <p>Any edges of a lot, parcel or Infill Block that do not abut a Street Type or Through Connection shall meet Development Standards for Side and Rear Yards as applicable.</p>		
Primary and Secondary Street Facing Frontage	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions.
Side Yard Setback	Residential: Min. 20 feet Commercial: Min. 0 feet	Residential: Min. 20 feet (1) Commercial: Min. 0 feet	Residential: Min. 20 feet Commercial: Min. 0 feet
Rear Yard Setback	Residential: Min. 20 feet Commercial: Min. 0 feet	Residential: Min. 20 feet (1) Commercial: Min. 0 feet	Residential: Min. 20 feet Commercial: Min. 0 feet
Minimum Building or Podium Height	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions. (1)	See Building and Landscape Frontage Types for applicable dimensions.
Maximum Podium Height	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions.
Required Step Back at Podium	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions.	See Building and Landscape Frontage Types for applicable dimensions.

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	Woodland Square	Pacific Avenue	Master Plan Areas
Tower Location	Tower spacing: Min. 65 feet between towers See Building and Landscape Frontage Types for additional applicable dimensions.	Tower spacing: Min. 65 feet between towers See Building and Landscape Frontage Types for additional applicable dimensions.	Tower spacing: Min. 65 feet between towers See Building and Landscape Frontage Types for additional applicable dimensions.
Maximum Building Height	See Regulating Plan – Building Heights for applicable dimensions.	See Regulating Plan – Building Heights for applicable dimensions.	See Regulating Plan – Building Heights for applicable dimensions.
Maximum Tower Floor Plate	Residential: 12,000 square feet Commercial: 35,000 square feet	Residential: 12,000 square feet Commercial: 35,000 square feet	Residential: 12,000 square feet Commercial: 35,000 square feet
Parking Structure	When a Parking Structure faces a Primary Street, it shall meet the requirements of one of the Permitted Frontage Types for the height of the Podium Front setback, Primary Street: Same as for Frontage Type When a Parking Structure faces a Secondary Street, it shall meet the requirements of one of the Permitted Building Frontage Types for the height of the Ground Floor Front setback, Secondary Street: 10 feet maximum Setback, side: Min. 20 feet Setback, rear: Min. 20 feet	When a Parking Structure faces a Primary Street, it shall meet the requirements of one of the Permitted Frontage Types for the height of the Podium Front setback, Primary Street: Same as for Frontage Type When a Parking Structure faces a Secondary Street, it shall meet the requirements of one of the Permitted Building Frontage Types for the height of the Ground Floor Front setback, Secondary Street: 10 feet maximum Setback, side: Min. 20 feet Setback, rear: Min. 20 feet	When a Parking Structure faces a Primary Street, it shall meet the requirements of one of the Permitted Frontage Types for the height of the Podium Front setback, Primary Street: Same as for Frontage Type When a Parking Structure faces a Secondary Street, it shall meet the requirements of one of the Permitted Building Frontage Types for the height of the Ground Floor Front setback, Secondary Street: 10 feet maximum Setback, side: Min. 20 feet Setback, rear: Min. 20 feet

(1) Proportional Compliance Adjustment 20 percent

TABLE 16.24.060-2, SITE DESIGN AND LANDSCAPE STANDARDS

	Woodland Square	Pacific Avenue	Master Plan Areas
SITE DESIGN AND LANDSCAPE			
Applicability	Areas bounded by designated Street Types (6 th Avenue, Golf Club Road, Pacific Avenue), Secondary Streets or Through Connections shall be designated as an Infill Block, lot or parcel and subject to the Development Standards for Site Design and Landscaping.		
Surface Parking	<p>Not permitted adjacent to a Primary Street</p> <p>When Surface Parking is located adjacent to a Secondary Street, it shall meet the requirements of one of the Permitted Landscape Frontage Types</p> <p>Front setback, Secondary Street: Min. 10 feet</p> <p>Setback, side: Min. 10 feet</p> <p>Setback, rear: Min. 10 feet</p>	<p>When Surface Parking is located adjacent to a Primary and/ or Secondary Street, it shall meet the requirements of one of the Permitted Landscape Frontage Types</p> <p>Front setback, Secondary Street: Min. 10 feet</p> <p>Setback, side: Min. 10 feet</p> <p>Setback, rear: Min. 10 feet</p>	<p>Not permitted adjacent to a Primary Street</p> <p>When Surface Parking is located adjacent to a Secondary Street, it shall meet the requirements of one of the Permitted Landscape Frontage Types</p> <p>Front setback, Secondary Street: Min. 10 feet</p> <p>Setback, side: Min. 10 feet</p> <p>Setback, rear: Min. 10 feet</p>
Side Yard Setback	<p>Residential: Min. 20 feet</p> <p>Commercial: Min. 0 feet</p>	<p>Residential: Min. 20 feet</p> <p>Commercial: Min. 0 feet</p>	<p>Residential: Min. 20 feet</p> <p>Commercial: Min. 0 feet</p>
Rear Yard Setback	<p>Residential: Min. 20 feet</p> <p>Commercial: Min. 0 feet</p>	<p>Residential: Min. 20 feet</p> <p>Commercial: Min. 0 feet</p>	<p>Residential: Min. 20 feet</p> <p>Commercial: Min. 0 feet</p>

16.24.070 Building and Landscape Frontage

Overview of Building and Landscape Frontage Types

Refer to the Regulating Plan and the Development Standards tables to determine which Building and Landscape Frontage Types are permitted along each Street. Each Street-facing Build To Line shall comply with the Development Standards listed under the applicable Building and Landscape Frontage Type.

Linear Building Frontage

A Linear Building Frontage, as set out in Figure 16.24.070-1, is characterized by a façade that is built up to the Build To Line. The building entrance is at sidewalk grade, except where there are ground floor residential uses. Linear Building Frontages have substantial glazing on the ground floor, and often provide awnings or canopies cantilevered over the sidewalk. Building entries must either provide a canopy or awning and/or be recessed behind the front building façade.

Forecourt Building Frontage

A Forecourt Building Frontage, as set out in 16.24.070-2, may be created by recessing a portion of the façade for a portion of the building frontage. The Forecourt Building Frontage should be used in conjunction with the Linear Building Frontage. A Forecourt Building Frontage is suitable for commercial or residential uses. A Forecourt Building Frontage may be suitable for gardens and/or outdoor seating.

Porch / Stoop / Terrace Building Frontage

The Porch-Stoop-Terrace Building Frontage, as set out in Figure 16.24.070-3, is characterized by a façade which is set behind the Build To Line and a building entry threshold, such as a porch or terrace, set between the building and the Build To Line. The threshold may be elevated above or sunken below grade. The building entry is accessed from this threshold. Landscaping may be provided in the setback area between the building and the sidewalk. A Porch-Stoop-Terrace Building Frontage is suitable for residential uses and service commercial or office uses.

Landscape Building Frontage

A Landscape Building Frontage, as set out in Figure 16.24.070-4, is set back from the Build To Line by a wide landscaped strip between the building and the sidewalk. This frontage type is appropriate along streets where the existing streetscape may not be conducive to pedestrian-oriented ground floor retail or residential, such as where there is no on-street parking or where streets are very wide. Ground floor entries must still be provided along and connected to the sidewalk.

Low Wall and Trellis Landscape Frontage

As set out in Figure 16.24.070-5, Build To Lines not occupied by buildings, driveways, or pedestrian paths must be screened with a low masonry or concrete wall and overhanging trellis structure.

Urban Wall or Fence Landscape Frontage

As set out in Figure 16.24.070-6, Build To Lines not occupied by buildings, driveways, or pedestrian paths must be screened with an open framework wall or fence of either metal, wood, masonry, or a combination.

Landscape Setback Frontage

As set out in Figure 16.24.070-7, Build To Lines not occupied by buildings, driveways, or pedestrian paths must be set back behind a planted landscape area consisting of trees, shrubs, and groundcover plants.

General Building and Landscape Frontage Standards

Applicability. The applicable Street Types or Sub-districts are stated at the top of each column. Development on any site adjacent to 6th Ave, Golf Club Road or Pacific Avenue shall conform to the regulations set out in the relevant column. Development on any site not adjacent to 6th Avenue, Golf Club Road or Pacific Avenue shall conform to the regulations set out in the Urban District, Master Plan District or Pacific Avenue District columns, as applicable.

Build To Line

Build To Line means the line up to which buildings or landscaping must be constructed. The **Build To Line** may not be the same as the **Front Lot Line**, see Table 16.24.050-2.

Frontage

Frontage shall be defined as the linear distance between centerlines of the perpendicular Secondary Street, Other Street or Through Connection, if measuring along a Primary Street.

If on a Secondary Street, Frontage shall be defined as the linear distance between centerlines of the perpendicular Other Street or Through Connection.

All other Frontage shall be defined as the linear distance between centerlines of the perpendicular Primary Street, Secondary Street, Other Street or Through Connection.

Where Frontage occurs on a curved segment of a street, Frontage shall be defined as the linear dimension of the Chord.

Frontage Requirements

Minimum Building Frontage along Street-Facing Build To Line: All private and public street or path-facing Build To Lines not occupied by buildings or driveways are required to provide Building or Landscape Frontage between the sidewalk and the remainder of the site.

Primary Street Frontage: The Primary Street Frontage shall be defined as the portion of the building facing the street (or the higher order street if on a corner). The front façade of the building shall be built to the Primary Street Frontage Build To Line.

Secondary Street Frontage: The Secondary Street Frontage shall be defined as the portion of the building facing the lower order Street, if on a corner. The front façade of the building shall be built to the Secondary Street Frontage Build To Line for a minimum of 100 feet from the corner or the lot width, whichever is shorter. The Building and Landscape Frontage Standards of this section shall apply to the portion of the building that occupies the Build To Line for 100 feet from the corner or the lot width, whichever is shorter.

Ground Floor Height Measurement. If a minimum Ground Floor Height is required, with a specific minimum floor to ceiling measurement, the ceiling shall be considered as the bottom of joists, rafters or supporting structure of the roof or floor structural system above; the floor shall be considered as the highest point of any flooring system. The ceiling does not include any non-structural ceiling surface materials such as suspended acoustical tile. Projections such as pendant lighting, exposed mechanical ducting, exposed electrical or communication raceways, or the bottom chord of structural trusses may extend below the ceiling and shall not be included in the floor to ceiling measurement.



Figure 16.24.070-1, Building Frontage – Linear

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TABLE 16.24.070-1, BUILDING FRONTAGE TYPE 1 – LINEAR

Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific Ave District	Pacific Ave
a Minimum Building or Podium Height	Min. 30 feet				Min. 20 feet	
	Maximum Podium Height					
Podium Setback from Build-To Line	Max. 10 feet		Max. 0 feet	Max. 10 feet	Max. 10 feet	
	Min. 0 feet		Min. 0 feet	Min. 5 feet	Min. 0 feet	
b Tower Step Back at Top of Podium	Min. 10 feet					
	Min.15 feet on lots adjacent to Golf Club Road					
	On Through Block Connections: 10 feet minimum Intermediate step back of 15 feet is required at a height of between 0 feet and 20 feet					
Tower Height	See Regulating Plan for Building Heights					
c Ground Floor Height	Min.18 feet					
Ground Floor Construction	1 hour fire resistive					
Ground Floor Depth	Min.40 feet					
Separation of Ground Floor Residential Uses	Vertical distance from ground: Min. 18 inches / Max. 3 feet Horizontal distance from Build To Line: Min. 3 feet / Max. 15 feet					

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Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific Ave District	Pacific Ave
d Weather Protection	Protected area: 50 square feet, minimum; 5 feet min. horizontal dimension; 10 foot vertical clearance, minimum				No requirement	Protected area: 50 square feet, minimum; 5 feet min. horizontal dimension; 10 foot vertical clearance, minimum
e Primary Entry Doors	Shall face street; 40% transparent min.					
Windows	60% min.					

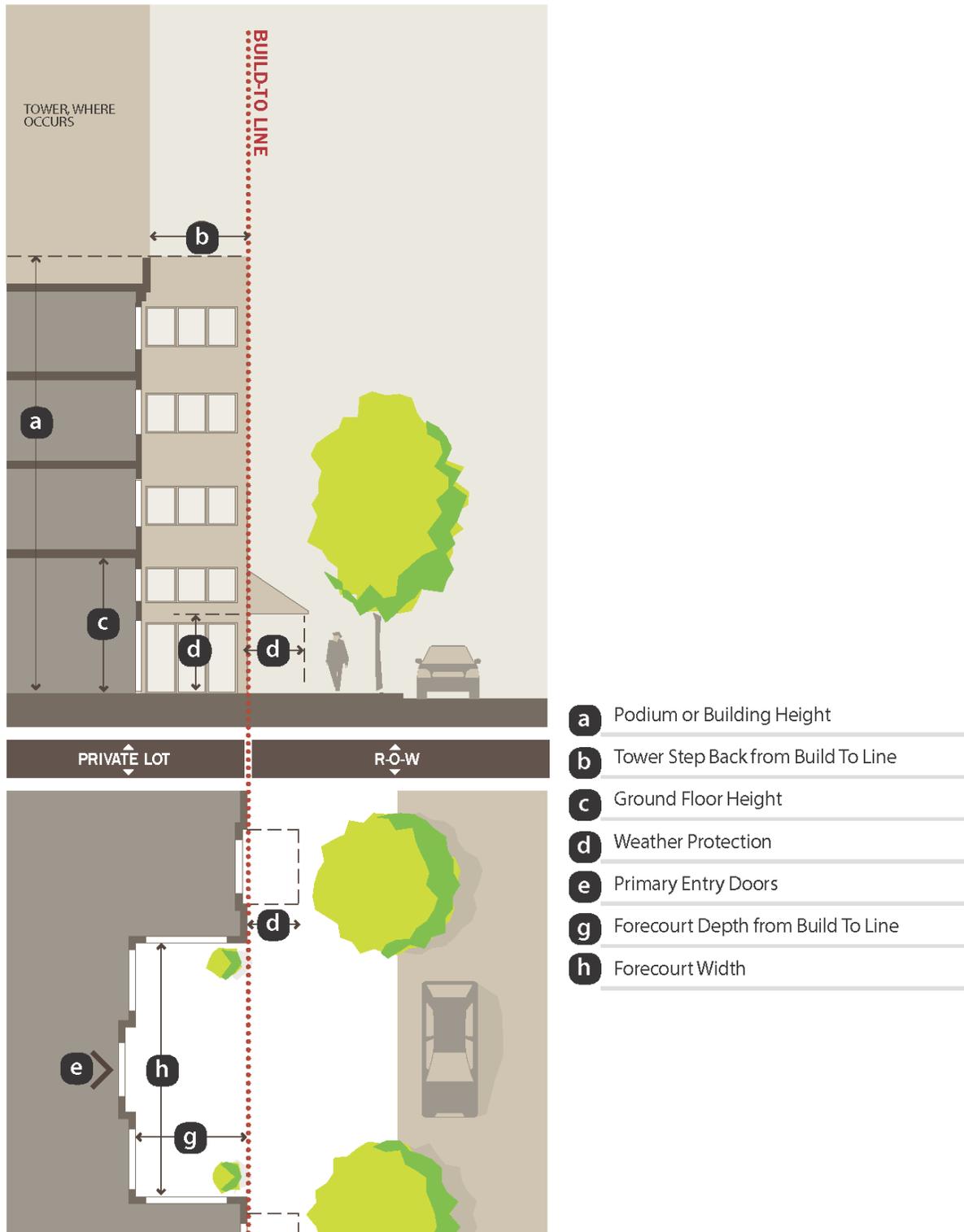


Figure 16.24.070-2, Building Frontage – Forecourt

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TABLE 16.24.070-2, BUILDING FRONTAGE TYPE 2 – FORECOURT

Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific District	Pacific Ave
a Minimum Building or Podium Height	Min. 30 feet					Min. 20 feet
	Maximum Podium Height					
	Max. 55 feet					
Podium Setback from Build-To Line	Max. 10 feet		Max. 0 feet	Max. 10 feet	Max. 10 feet	
	Min. 0 feet		Min. 0 feet	Min. 5 feet	Min. 0 feet	
b Tower Step Back at Top of Podium	Min. 10 feet or match					
	Min. 15 feet on lots adjacent to Golf Club Road					
	On Through Block Connections: 10 feet minimum Intermediate step back of 15 feet is required at a height of between 0 feet and 20 feet					
	Tower setback shall match Forecourt maximum depth where tower abuts the Forecourt					
Tower Height	See Regulating Plan for Building Heights 16.24.040-3 Regulating Plan, Heights					
c Ground Floor Height	Min. 18 feet					
Ground Floor Construction	1 hour fire resistive					
Ground Floor Depth	Min. 40 feet					
Separation of Ground Floor Residential Uses	Vertical distance from ground: Min. 18 inches / Max. 3 feet					
	Horizontal distance from Build To Line: Min. 3 feet / Max. 15 feet					

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Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific District	Pacific Ave
d Weather Protection	Protected area: 50 square feet, minimum; 5 feet min. horizontal dimension; 10 foot vertical clearance, minimum				No requirement	Protected area: 50 square feet, minimum; 5 feet min. horizontal dimension; 10 foot vertical clearance, minimum
e Primary Entry Doors	Shall face street; 40% transparent min.					
Windows	60% min.					
g Forecourt Depth from Build To Line	Setback: 10 feet minimum; 30 feet maximum Tower setback shall match Forecourt maximum depth					
h Forecourt Width	Setback: 10 feet minimum; 30 feet maximum					
Forecourt Frontage	The Forecourt Frontage shall incorporate the Linear Frontage Type for building faces on the Primary and Secondary Street Frontages that are not part of the courtyard.					
Fence	No greater than 3 feet in height; min. 20% transparent					

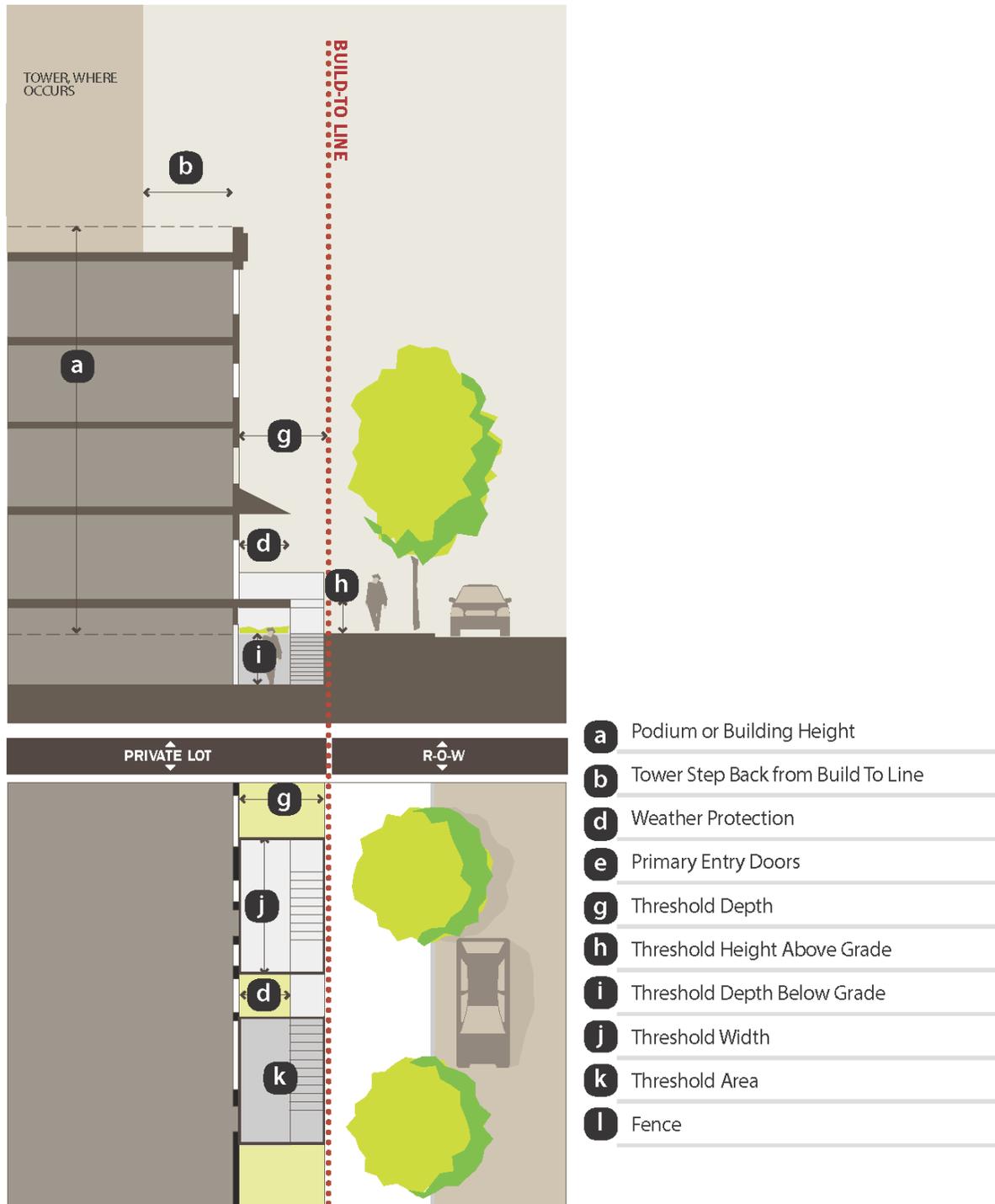


Figure 16.24.070-3, Building Frontage – Porch-Stoop-Terrace

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TABLE 16.24.070-3, BUILDING FRONTAGE TYPE 3 – PORCH-STOOP-TERRACE

Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific District	Pacific Ave
a Minimum Building or Podium Height	Min. 30 feet				Min. 20 feet	
	Maximum Podium Height					
Podium Setback from Build-To Line	Max. 15 feet		NA	Max. 15 feet		
	Min. 5 feet			Min. 5 feet		
b Tower Step Back at Top of Podium	Min. 10 feet					
	Min. 15 feet on lots adjacent to Golf Club Road					
	On Through Block Connections: 10 feet minimum Intermediate step back of 15 feet is required at a height of between 0 feet and 20 feet Tower setback shall match Threshold maximum depth					
Tower Height	See Regulating Plan for Building Heights 16.24.040-3 Regulating Plan, Heights					
c Ground Floor Height	Min. 18 feet					
Ground Floor Construction	1 hour fire resistive					
Ground Floor Depth	Min. 40 feet					
Separation of Ground Floor Residential Uses	Vertical distance from ground: Min. 18 inches / Max. 3 feet					
	Horizontal distance from Build To Line: Min. 3 feet / Max. 15 feet					

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Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific District	Pacific Ave
d Weather Protection	Protected area: 20 square feet, minimum; 5 feet min. horizontal dimension; 10 foot vertical clearance, minimum				No requirement	Protected area: 20 square feet, minimum; 5 feet min. horizontal dimension; 10 foot vertical clearance, minimum
e Primary Entry Doors	Shall face street; 20% transparent min.					
Windows	30% min.					
g Threshold Depth	Min. 4 feet					
h Threshold Height Above Grade	Max. 5 feet					
i Threshold Depth Below Grade	Max. 4 feet					
j Threshold Width	Min. 5 feet					
j Threshold Area	Max. 150 Square feet per building entry					
k Fences	No greater than 3 feet in height; min. 20% transparent					

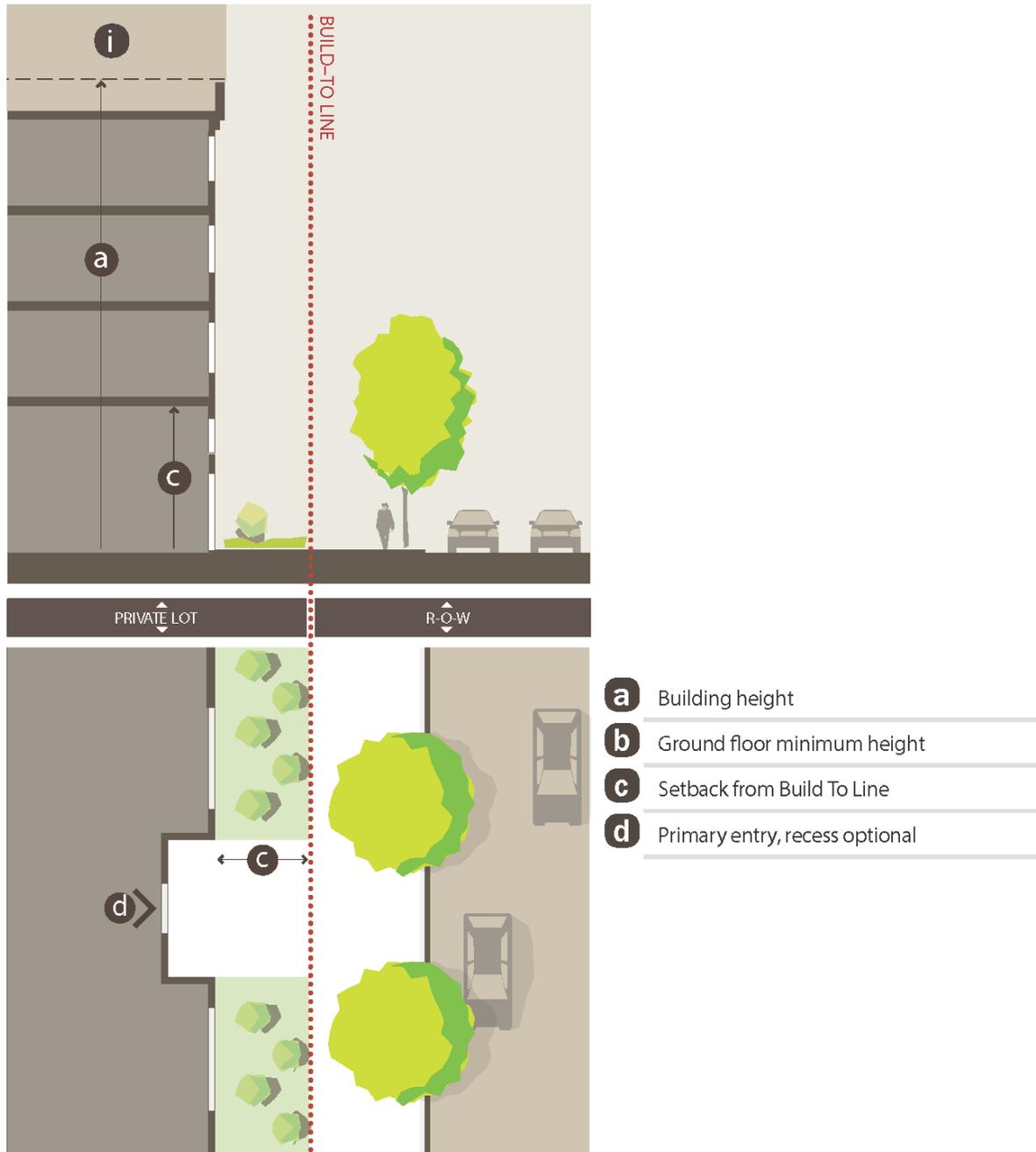


Figure 16.24.070-4, Landscape Building

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TABLE 16.24.070-4, BUILDING FRONTAGE TYPE 4 – LANDSCAPE BUILDING

Development Standard	Woodland Square	Master Plan District	Pacific District	Pacific Ave
a Minimum Building or Podium Height	Min. 30 feet		Min. 20 feet	
Maximum Podium Height	Max. 55 feet			
c Podium Setback from Build-To Line	Max. 15 feet Min. 10 feet			
Tower Step Back at Top of Podium	Min. 10 feet Min.15 feet on lots adjacent to Golf Club Road On Through Block Connections: 10 feet minimum Intermediate step back of 15 feet is required at a height of between 0 feet and 20 feet Tower setback shall match Threshold maximum depth			
Tower Height	See Regulating Plan for Building Heights 16.24.040-3 Regulating Plan, Heights			
Minimum Building Depth	Min.40 feet			
Weather Protection	Building entrances shall be either be covered by an awning or canopy or be covered by being recessed behind the front building façade. If an awning or canopy is provided, it must provide a minimum vertical clearance of 8 feet and a maximum clearance of 15 feet. If only a recessed entry is provided, it must be recessed behind the front facade a minimum of 3 feet and a maximum of 5 feet.			
d Primary Entry Doors	At least one building entrance shall be directly connected to the Primary or Secondary Street with a walkway measuring a minimum of 5 feet wide. A minimum of 40% of each primary entry shall be transparent.			
Windows	Transparent ground floor windows must be provided along a minimum of 60% of the ground floor, Primary and Secondary Street-facing façade area.			
Service and Utility Equipment	Building service and utility equipment and outdoor storage of garbage and/or recycling is not permitted along a Primary or Secondary Street or within the required setback from Build-To Line.			

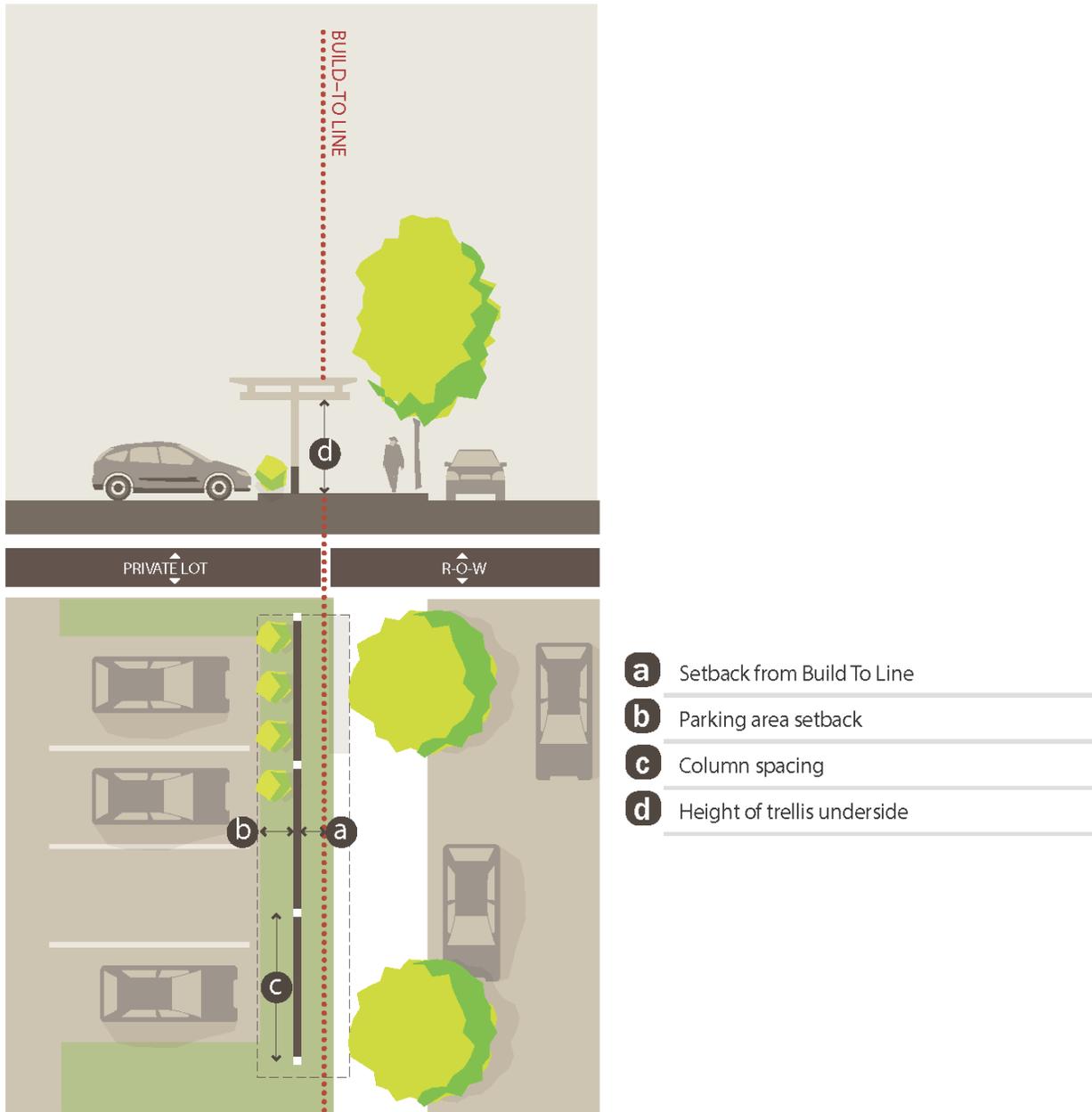


Figure 16.24.070-5, Low Wall and Trellis

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TABLE 16.24.070-5, BUILDING FRONTAGE TYPE 5 – LOW WALL AND TRELLIS

Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific District	Pacific Ave
Frontage	Where specified according to Table 16.24.050-1 (Streets, Through Connections and Connection Spacing) and Table 16.24.050-2 (Street and Through Connection Types), surface parking areas shall be screened with a Low Wall and Trellis along the Build-To Line.					
a Setback from Build To Line	The Low Wall and Trellis shall be set back a minimum of 0 feet and a maximum of 5 feet from the Build-To Line.					
c Column Spacing	The Trellis shall have masonry, heavy timber, or steel (or similar metal) supporting columns spaced no more than 30 feet on center.					
d Height of Trellis	The underside of the Trellis portion of a Low Wall and Trellis shall be a minimum of 8 feet above grade and a maximum of 14 feet above grade. The Trellis shall be heavy timber or steel (or a similar material) and shall consist of open structure with no decking or awning material.					
Low Wall	The Low Wall portion of a Low Wall and Trellis shall be a minimum of 1.5 feet and a maximum of 3 feet and have a minimum depth of 1.5 feet. The Low Wall shall be wood, masonry, and/or concrete.					
Low Wall Openings	Openings in the Low Wall and Trellis are allowed for pedestrian pathways, sidewalks, plazas, and driveways.					
b Surface Parking Setback	Surface Parking shall be set back a minimum of 3 feet from the Low Wall and Trellis.					
Ground Cover	<p>Any setback area between the sidewalk and the wall shall be planted or paved with stamped concrete or masonry pavers.</p> <p>The setback between the Low Wall and surface parking shall be planted with low shrubs, groundcover, and climbing plants.</p>					

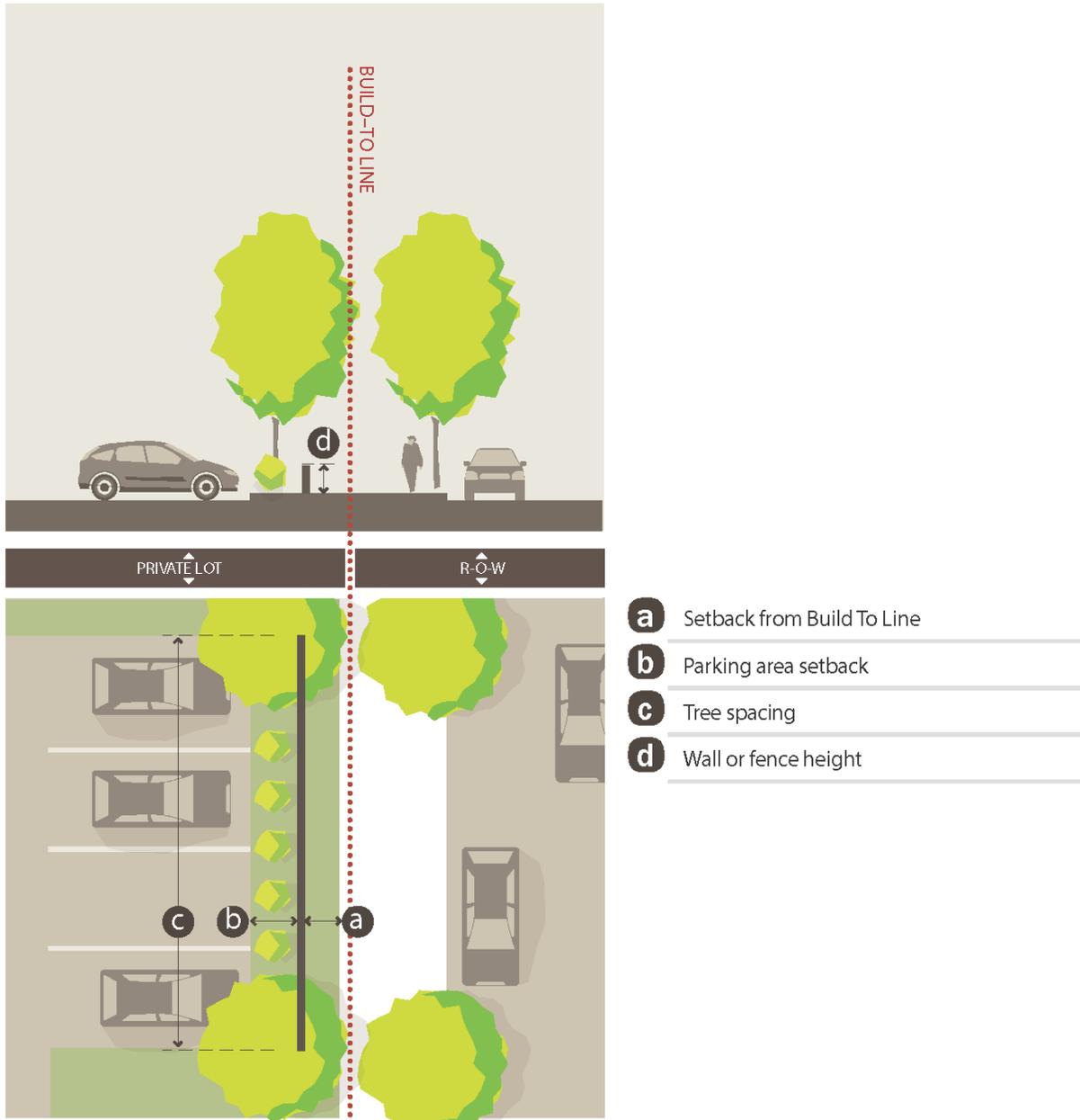


Figure 16.24.070-6, Urban Fence or Wall

TABLE 16.24.070-6, BUILDING FRONTAGE TYPE 6 – URBAN FENCE OR WALL

Development Standard	Woodland Square	Master Plan District	6 th Ave	Golf Club Rd	Pacific District	Pacific Ave
Frontage	Where specified according to Table 16.24.050-1 (Streets, Through Connections and Connection Spacing) and Table 16.24.050-2 (Street and Through Connection Types), surface parking areas shall be screened with an Urban Fence or Wall along the Build-To Line.					
a Setback from Build To Line	The Urban Fence or Wall shall be set back a maximum of 5 feet from the sidewalk. The area between the Urban Fence or Wall shall be hardscaped with either masonry pavers or stamped concrete.					
d Wall or Fence Height	Walls shall be wood masonry, and/or concrete; fences shall be made of wrought iron, steel, or a similar material (but not chain-link) and must be dark in color. The fence shall be at least 2 feet high and no more than 3 feet high. Fences may be no more than 50% sight obscuring. The wall shall be at least 2 feet high and no more than 3 feet high.					
c Tree Spacing	In addition to the required fence or wall, trees and shrubs shall be provided. One large tree is required every 30 linear feet minimum along all public or private Street-facing frontages, except where it is necessary to ensure adequate traffic visibility. The shrubs shall be at least as high as the wall or fence, and shall be no more than 6 feet high.					
Wall or Fence Openings	Openings in the Urban Fence or Wall are allowed for pedestrian pathways, sidewalks, plazas, and driveways.					
b Surface Parking Setback	The surface parking area shall be set back, at a minimum, an additional 5 feet to provide room for required landscaping and stormwater infiltration and/or retention.					
Ground Cover and Planting	Ground cover plants must fully cover any remaining landscaped area between the parking area and the Urban Fence or Wall.					

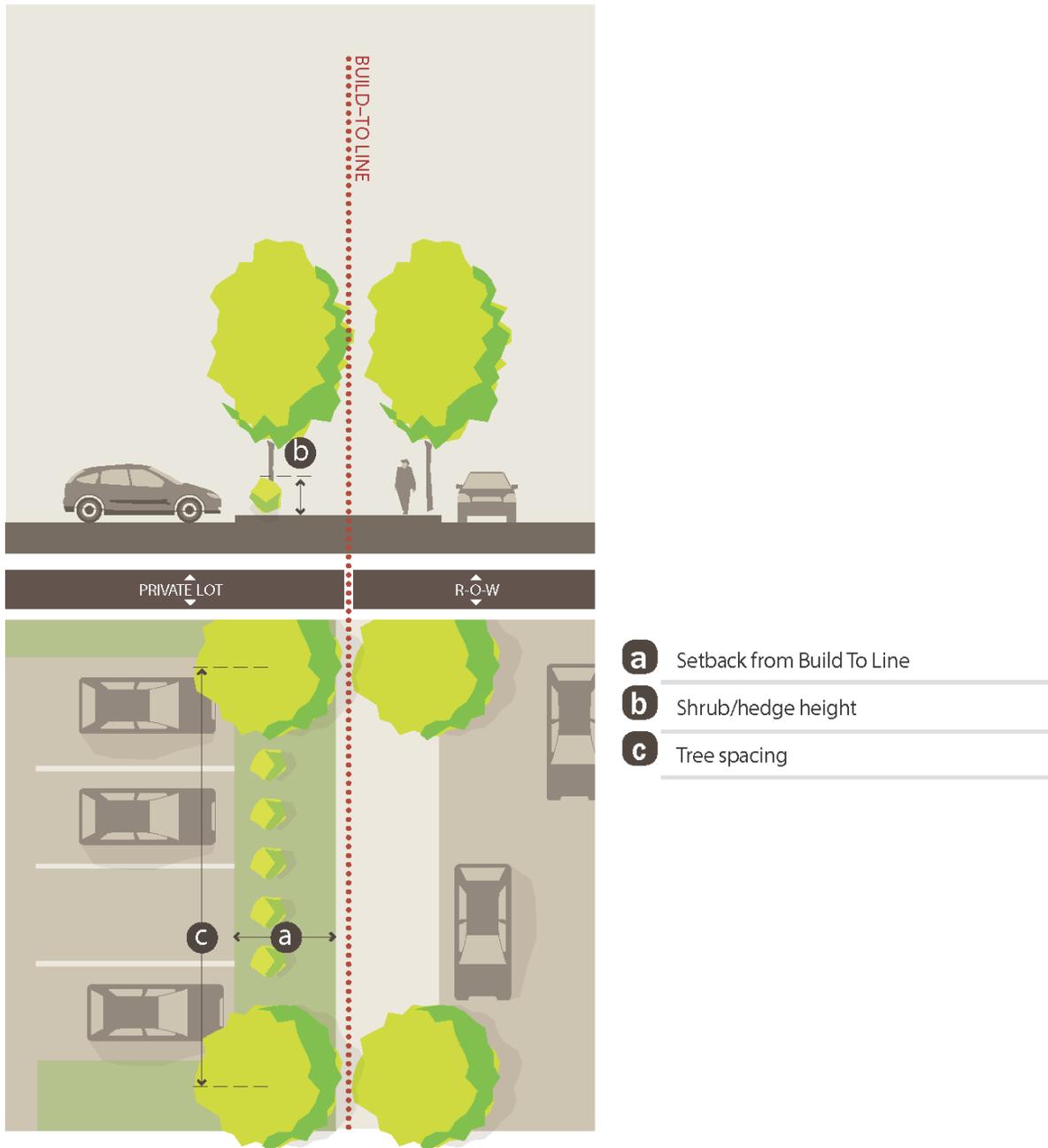


Figure 16.24.070-7, Landscape Setback

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TABLE 16.24.070-7, BUILDING FRONTAGE TYPE 7 – LANDSCAPE SETBACK

Development Standard	Woodland Square	Master Plan District	Pacific District	Pacific Ave
Frontage	Where specified according to Table 16.24.050-1 (Streets, Through Connections and Connection Spacing) and Table 16.24.050-2 (Street and Through Connection Types), surface parking areas shall be screened with an Landscape Setback along the Build-To Line.			
a Setback from Build To Line	Along all public or private Street-facing frontages, surface parking shall be set back a minimum of 10 feet behind the Build-To Line.			
b Shrub and Hedge Height	<p>The surface parking area shall be screened with a continuous row of hedges or shrubs immediately adjacent to the parking area, except where there is a driveway. The shrubs shall be a minimum of 3 feet high and must be mostly opaque year round.</p> <p>A 3 feet high masonry wall may be substituted for the shrubs but the trees and groundcover plants are still required.</p>			
c Tree Spacing	In addition to the required shrubs, one large tree is required every 30 linear feet minimum along all public or private Street-facing frontages. The shrubs/hedge shall be interrupted with a gap of up to 2 feet wide in order to accommodate trees.			
Walkways	Openings in the Setback are allowed for pedestrian pathways, sidewalks, plazas, and driveways.			
Ground Cover and Planting	Grass or ground cover plants must fully cover the remainder of the landscaped area between the parking area and the sidewalk.			

16.24.100 Development Review

16.24.100 Development Review Process

- A. Administration of development review shall be the responsibility of the director of community development. The director shall implement development review concurrent with any related planning review process or building permit application. The development review process consists of a presubmission conference followed by an application review by either the director or the Site Plan Review Committee (SPRC).
1. Presubmission Conference
 - a. The applicant shall attend a presubmission conference with an assigned staff member. The presubmission conference shall be conducted in accordance with Section 1B.020 of the City of Lacey Development Guidelines and Public Works Standards.
 - b. The site plan, landscaping plan, and building design may be conceptual in form for the presubmission conference.
 - c. Staff shall provide a written summary of the meeting to the applicant including identification of the relevant approval criteria in Sections 16.24.060 through 16.24.100.
 - d. The staff summary shall identify the applicable review process, which will be an administrative review by the director of community development or a review by the SPRC.
 - e. The staff summary shall identify any submittal requirements in Section 16.24.130 that are not applicable or required.
 2. Limited Administrative Review
 - a. Development applications, which do not exceed the thresholds in Section 16.24.100 B, shall be subject to an administrative review by the director under LMC 12.28 Development Standards and Public Works Standards.
 - b. The director shall conduct this review concurrent with any related planning review process or building permit application.
 3. Review by the SPRC
 - a. Development applications, which exceed the thresholds in Section 16.24.100 B, shall be subject to a review by the SPRC.
 - b. The SPRC shall review development applications in accordance with the full administrative review process and timelines outlined in Section 1C.040 of the City of Lacey Development Guidelines and Public Works Standards. The application shall be approved or approved with conditions to conform to the standards, provisions and policies of the city as expressed in its various adopted plans and ordinances. Whenever the SPRC disapproves an application, it shall set forth in writing its findings, which shall specify the particular standards, provisions and policies to which the site plan fails to conform and the reasons why it fails to conform.
 - c. The site plan review committee (SPRC) shall have the prerogative of refusing to rule on a development application if in the opinion of the SPRC the site plan is sufficiently complex that it should be reviewed by the hearings examiner according to the quasi-judicial process in Section 1C.050 of the City of Lacey Development Guidelines and Public Works Standards. The SPRC shall decide to transfer review authority to the hearings examiner within fourteen days of the Determination of Completeness, according to Section 1B.070 of the City of Lacey Development Guidelines and Public Works Standards.
- B. Development review and approval by the SPRC shall be required for any of the following activities:
1. The use of land for the location of any commercial, industrial or public building or activity, and for the location of any building containing more than two dwelling units or lot with more than one residential structure other than a permitted accessory dwelling.

2. A change of land use at an existing site or structure when the new activity requires either a change of occupancy according to the Building Code or is a change of land use according to the Standard Industrial Classification code and, in the opinion of the community development director, results in an intensification of land use and will require new development conditions to comply with existing regulations. This provision may not apply to malls (buildings with ten or more tenants sharing common parking) where original conditions to establish the mall complex anticipated a range of tenants and existing facilities and where it can be shown existing infrastructure can accommodate the new intensified use.
3. Expansion of an existing commercial, industrial, public or multifamily structure or use. Provided residential duplexes are exempt.
4. A remodel of an existing structure where the remodel is twenty-five percent or more of the assessed valuation of existing structures. The remodel value shall be calculated according to methodology described in Chapter [14.04](#) LMC adopting the Building Code. The value of existing structures shall be the most recent value assigned by the County Assessor. The twenty five percent threshold shall be cumulative over the most recent five years, including calculations of all previously exempt remodels, but shall not include life/safety improvements or normal maintenance not requiring a building permit. Remodels of residential duplex, triplex, and quadraplex shall be exempt from site plan review.
5. Uses and activities within designated environmentally sensitive areas or their buffers pursuant to the requirements of LMC Title [14](#).

16.24.110 Appeals

Any decision of the city of Lacey in the administration of Chapter 16.24 LMC may be appealed in accordance with Section 1D.010 Appeals of the City of Lacey Development Guidelines and Public Works Standards.

16.24.120 Amendment of an Approved Development Application.

- A. A development approval granted approval by the director, SPRC, hearings examiner or by the city council may be amended. If, in the opinion of the director of community development, the modifications are considered minor, no additional review process shall be required. If the modifications are considered significant by the director of community development, then the site plan shall be modified by the same procedures provided under LMC 16.24.100.

16.24.130 Development Review – Submittal Requirements.

The development application shall contain the following items:

- A. Application narrative. Four copies required.
 - a. Project site address;
 - b. Project description;
 - c. List of requested adjustments, if any;
 - d. List of submittals provided;
 - e. For all multifamily projects or mixed-use projects with multifamily development, provide a description of compliance with crime prevention through environmental design (CPTED) techniques;
- B. Plans. Four copies of the set of plans are required. The license stamps of the architect and landscape architect shall be on each appropriate plan page.
 1. Vicinity Plan. A vicinity plan is required containing the following information (1 inch equals 500' or larger):
 - a. Site boundaries
 - b. Site address;
 - c. Woodland District Neighborhood designation;
 - d. Plan showing project location within the Woodland District;
 - e. Names of adjacent streets with corridor designation (Refer to § 16.24.040-2 Regulating Plan, Streets); and

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- f. Names and descriptions of frontage types required and proposed.
2. Site Plan. A site plan is required containing the following information (1 inch equals 20' or larger):
 - a. Site address;
 - b. Woodland District Neighborhood designation;
 - c. Names of adjacent streets;
 - d. Location of adjacent buildings on abutting properties;
 - e. Site dimensions;
 - f. Existing grade and finished grade (maximum two-foot contours);
 - g. Location and dimensions of existing and proposed site circulation for automobiles and pedestrians. Indicate location of site ingress and egress and patterns of on-site automobile circulation with directional arrows. Clearly identify any requested adjustments to development standards (Refer to § 16.24.050 Streets, Through Connections and Connection Spacing);
 - h. Location and dimensions of existing and proposed structure(s), accessory structures with appropriate setbacks;
 - i. Location of trees as determined by the Lacey tree protection professional;
 - j. Location, dimensions, and nature of any proposed easements or dedications; and
 - k. Location, dimensions, and description of common open space and recreation areas (Refer to § 16.24.030 General Standards Required for All Development).
 3. Landscaping Plan. The landscaping plan shall contain the following information (1 inch equals 20' or larger):
 - a. Survey of existing trees; trees to be retained; and trees to be removed;
 - b. Existing plant material to be retained;
 - c. Proposed plant material to be placed on site. The type, size, number and spacing on plantings must be illustrated (Refer to § 16.24.030 General Standards Required for All Development);
 - d. Surface parking location and design (Refer to Chapter 16.72 LMC);
 - e. Bicycle parking location and design (Refer to Chapter 16.72 LMC);
 - f. Loading and Service Areas location and design (Refer to Chapter 16.80 LMC);
 - g. Screening and Buffering: general; perimeter fencing and walls; parking structures; and surface parking lots. (Refer to Chapter 16.80 LMC).
 4. Building Form and Massing. Submit complete elevations (1/8 inch equals 1' or larger) of all proposed construction and related elevations of existing structures (if any) within 25 feet of the site. Elevations shall include the following information:
 - a. Dimensioned elevations of building showing:
 - i. Required building setbacks (if any) (Refer to § 16.24.060, Building Form, Site Design and Massing);
 - ii. Required ground floor height (Refer to § 16.24.060, Building Form, Site Design and Massing);
 - iii. Required weather protection (Refer to § 16.24.060, Building Form, Site Design and Massing);
 - iv. Required ground floor transparency (Refer to § 16.24.060, Building Form, Site Design and Massing);
 - v. Required weather protection for required building entrance(s) (Refer to § 16.24.060, Building Form, Site Design and Massing);
 - vi. Pedestrian protection – sidewalk (Refer to § 16.24.060, Building Form, Site Design and Massing);
 - vii. Minimum, maximum, and proposed podium height (Refer to § 16.24.060, Building Form, Site Design and Massing); and
 - viii. Maximum building height and required building setbacks (if any) (Refer to § 16.24.060, Building Form, Site Design and Massing).

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- b. Elevations should show the type and color of exterior materials;
 - c. Location and elevations of exterior lighting for site and buildings; and
 - d. Perspective drawings, photographs, color renderings or other graphics which accurately represent the proposed project.
5. Sections. Submit a minimum of two site and building cross section profiles (1/8 inch equals 1' or larger) with the following information:
- a. Scale;
 - b. Building(s) details;
 - c. Landscaping against the building when installed;
 - d. Lighting fixtures and standards; and
 - e. Signs.
6. Roof plan. The roof plan shall contain the following information (1 inch equals 20' or larger):
- a. Extent of the project site and location of new and existing buildings;
 - b. Extent and location of new roof(s);
 - c. Extent and location of building tower(s) (if any);
 - d. Dimensions and area of floor plate for proposed building tower(s). If more than one tower, show clear dimensions between towers (Refer to § 16.24.060, Building Form, Site Design and Massing).

16.24.140 Additional information for review.

The SPRC, hearings examiner or city council may require the applicant to submit any additional information or material which it finds is necessary for the proper review and hearing of the application.